

Cornell Local Roads Program

Town of Ticonderoga, NY

2016



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I. Introduction

The Cornell Local Roads Program Center provides training, technical assistance, and information to municipal officials and employees responsible for the maintenance, construction, and management of local highways and bridges in New York State. This educational service has been widely used throughout New York State by many municipalities who wish to run their highway departments more effectively and efficiently.

Part of the Cornell Local Roads Program (CLRP) is a cost analysis tool developed by Civil Engineers from Cornell University. The purpose of this tool is to generate a road database for prioritizing and recommending various repairs objectively. The basis of the program is to collect road condition data and catalog a detailed list of information through software known as the Cornell Asset Management Program - Roads & Streets (CAMP-RS). CAMP-RS functions chiefly by “Keeping good roads good” and consistently improving the conditions of subpar roads within the constraints of a limited budget. CAMP-RS outputs a prioritized list of repairs in order to efficiently raise the condition of all roads to an acceptable level. In order to assist in the categorizing process of roads, sections are rated upon current surface condition, traffic volume, and communal importance.

This report will highlight:

1. A survey collection of current road conditions ordered by priority.
2. A list and definition of used repair categories.
3. A Ticonderoga municipality road map.
4. A distribution summary of Ticonderoga road conditions.
5. A collection of blank template documents for surveying road sections.

A. Background of Roads

Although road systems are used everyday by the majority of America, the process to construct a road is decidedly more complex than laying down some asphalt.

Roads are constructed in order to maintain a smooth, safe, and long lasting surface in which to travel on. The muddy, unpaved roads of the 19th century would not be suitable for a 40 ton tractor trailer truck. Modern roads are constructed to distribute the weight of traffic onto the native earth below.

A road's cohesive strength depends on adequate drainage and a solid foundation. Any water on the road is carried away by the crown (slanted road surface), over the shoulders, and is carried away by any ditches that are present. The base and subbase make up the foundation. The subbase is created by stabilizing the in-place soils through compaction or by mixing in an asphalt emulsion designed to strengthen the load bearing portion of the road. Above this, a base of aggregate--or of hot mix asphalt for heavy load bearing roads--is placed down to properly support the wheel loads and distribute the weight across a larger area of the subbase. The asphalt surface consists of a mixture of aggregates and asphalt cement that provides motorists with a smooth, durable surface that efficiently drains excess water off of the roadway.

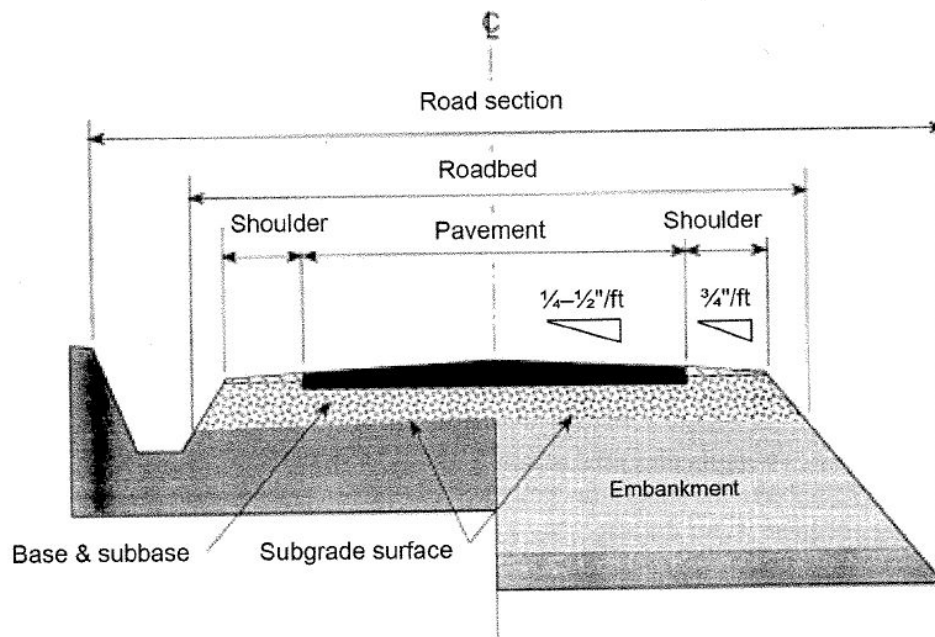


Figure 1. Typical pavement cross section

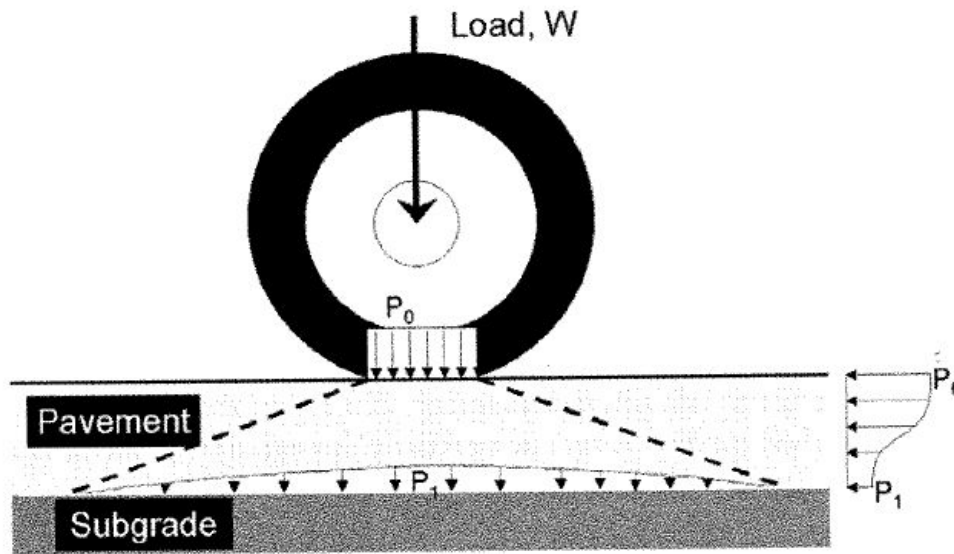


Figure 2. Spread of wheel load pressure through the pavement structure.

There are some popular misconceptions about the physical properties of roads; many believe roads to be solid, rigid, and unmovable. In reality, asphalt paved roads are very flexible. Compared to road pavement, a rigid material would break significantly faster as it would be unequipped to accommodate the frost heaving that is prevalent in the spring. Even when supporting traffic, the road surface bends and shapes slightly when distributing pressure to the subbase.

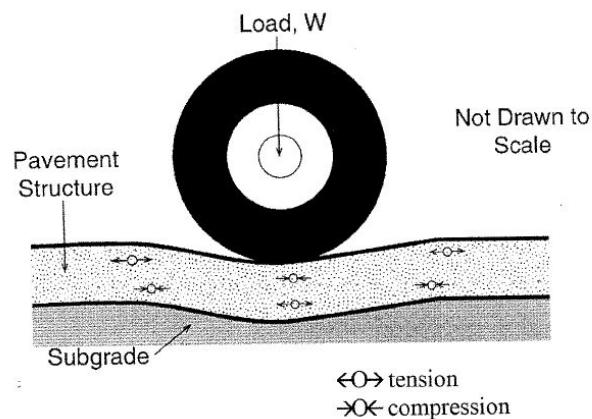


Figure 3. Pavement deflection under load

B. Fundamentals of a Good Road

At the training session, Cornell instructors Geoffrey Scott and David Orr made sure to emphasize how to define the elements of a well-constructed road. Roads should be able to withstand factors such as; temperature fluctuations, vehicle loads, seasonal changes, and improper building materials/procedures. The “Ten Commandments of a Good Road” consists of the following:

1. Get Water Away From the Road

The importance of drainage cannot be overemphasized in road construction and maintenance. The presence of improper or inadequate drainage can negatively impact a road's condition and lifespan by unsettling the base, weakening the surface, and causing a breakdown of pavement edges. Standing water and erosion from runoff can also contribute to these issues. Proper road drainage consists of these components:

- Proper Road Crown--Allows stormwater to run off of the roadway.
- Shoulders--Consists of the area between road surface and ditches that allows for quick, efficient movement of water.
- Ditches--Must carry and direct water away from the roadway, sometimes with the use of culverts.
- Culverts--Channels water from one side of the road to the other to transport water to a less problematic location.

2. Build On A Firm Foundation

The base supports everything above it, including traffic. Without adequate support, the road's condition will deteriorate rapidly. A strong foundation ensures a fundamentally sound and long-lasting road. A good foundation should consist of a stable material (Item 4) that does not deform excessively under repeated loads and times of varying moisture content.

3. Use The Best Materials Available

Since most towns have to work around a tight budget, the cost of materials is a large contributing factor when deciding the affordability of various repairs. While cheaper soils and aggregates may

initially prove themselves as satisfactory, long-term consequences must be carefully considered. The use of inferior base materials may require excessive maintenance and an eventual expensive rehabilitation.

4. Compact All Materials Well

The tighter or denser a material is compacted, the stronger it will be. Properly compacting materials will lead to less air space and a stronger road overall. Well-graded soils that have a variety of particle sizes will compact to form a strong, resilient base.

5. Design For Winter Maintenance

Areas--like Ticonderoga--that receive substantial snowfall must design roads with winter maintenance in mind. Consider that roads should...

- Be wide enough to accommodate the opposing passage of snowplows and school buses.
- Be wide enough to store plowed snow, as well as handle spring thaws and heavy water flows.
- Have a longitudinal grade of at least one percent for drainage purposes and no greater than ten percent for safety reasons.
- Have a sight distance of 75-100 feet per additional 10 mph. This rule of thumb allows for adequate observation of potential upcoming hazards.

6. Build For Traffic Loads And Volumes

It is essential to identify the main uses of the road before it is constructed. For example, roads that service areas of industry should have thicker asphalt and base layers than residential roads in order to accommodate heavy vehicles and machinery. A general rule of thumb is to design for the worst case scenario or largest vehicle that will use the road. One 18-wheeler traveling over a roadway imparts as much damage as approximately 10,000 cars traveling over the same stretch. Heavy machinery can quickly destroy roads designed for strictly residential traffic.

7. Pave Roads Only When They Are Ready

Unpaved roads must be properly prepared before paving. Base materials of paved roads contain less fines and moisture than dirt or gravel roads. Ensuring that the base is constructed with well compacted free-draining soil is essential.

8. Build From The Bottom Up

Roadways that lack a good base layer will not receive any long term benefit from merely a resurface or overlay. It is necessary in many instances for old roads to be completely dug out in order to repair the underlying problem. Such problems could encompass improper drainage, insufficient base depth, or poor material quality. Issues with the base must be corrected before allocating funds to repair the surface.

9. Protect your Investment

Roads and bridges need regular preventive maintenance to prolong their lifespan. Neglecting this maintenance will likely result in extremely expensive rehabilitation. Preventive maintenance can include these repairs:

- Roadway Surfaces--patching, resurfacing, dust control, snow removal.
- Drainage--cleaning/repairing culverts and ditches.
- Roadside--cutting brush/grass, grooming shoulders, repairing erosion
- Bridges--repair of railing and decking, channel clearing

Creating a system where these activities are routine will lead to safe, effective, and long-lasting roadways that will ultimately save money in the long term.

10. Keep Good Records

It is imperative for each municipality to keep a good, well-documented record of local road information and conditions. Good record-keeping makes road work easier for everyone involved. It will be easier for the Highway Department to draw up budgets and to show citizens future plans for roadwork. Road information such as surface type, shoulder type, width, length,

problem areas and last known repairs should be collected along with the current road condition. This data can be used to efficiently prioritize needed improvements.

II. CAMP-RS Process

The Town of Ticonderoga Highway Department hired Colvin Chapman and Samuel Shelmidine as pavement management interns for the summer of 2016 to participate in the Cornell Local Roads Program. The interns--as well as Joe Giordano and Fred Hunsdon--attended a training session at Cornell University from May 31st, 2016 to June 2nd, 2016 that gave an overview of the program.

A. CAMP-RS Training at Cornell

The three day training session was focused upon learning the fundamentals of a good road, proper evaluation procedures, repair analysis, and CAMP-RS software operation. This training period also served to illustrate how the program can be tailored to accommodate the differences between municipalities within the state. At the end of the training, each town received a folder containing reference booklets as well as example documents related to implementing a proper Pavement Management System. On the last day, each town received a copy of the CAMP-RS software to take back to their municipality.

B. Road Surveying

Local roads are divided into sections according to existing county records. Segments typically begin and end at intersections.

Road surveying is the process of recording the characteristics of each section to determine the condition, appropriate maintenance, and repair. Each survey contains information such as the surface material, length, width, number of lanes and shoulder type.

Additionally, roads were ranked with regard to their traffic and importance on a scale from 1 to 5. For example, a remote road accessing few houses would typically have a traffic and importance ranking closer to 1, while roads near a school or hospital would have traffic and importance rankings of either 4 or 5.

Surveys consist of assigning values to each of the following distresses: longitudinal or transverse cracking, alligator cracking, edge cracking, patching and potholes, rutting, bleeding, drainage, and overall roughness. These conditions are given a rating for their severity (Low, Medium, High) and extent (Low, Medium, High).

Surveys were completed by driving or walking the length of each road section and noting the condition of each of the above categories. The data was then input into the software provided by the Cornell Program: CAMP-RS. The Ticonderoga municipality has 47.12 centerline miles and 94.8 lane miles of local roads.

C. Pavement Distresses

Longitudinal/Transverse Cracks



Severity-- Low



Medium



High

Longitudinal and transverse cracking is primarily caused by fatigue failure from repeated traffic loading and thermal movement, such as freezing and thawing. The crack itself will either be a straight line running parallel(longitudinal cracking) or perpendicular(transverse cracking) to the road.

Alligator Cracks



Severity-- **Low**

Medium

High

Similar to longitudinal cracks, alligator cracks typically originate from traffic loading or inadequate structure. However, alligator cracking can also be caused by insufficient drainage where water can seep in and weaken the roadway structure. Alligator cracking is a precursor to potholes and should be caught and repaired promptly.

Edge Cracking



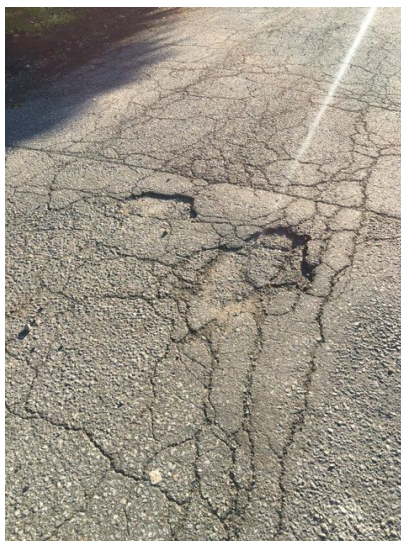
Severity-- **Low**

Medium

High

Edge cracking forms on the side of roadways and is caused by either lack of shoulder support or drainage issues. This distress often resembles alligator cracking that originates from the pavement edge and extends into the roadway.

Patching and Potholes



Extent-- **Low**

Medium

High

Potholes are often caused by alligator cracking, poor drainage/base, or too thin of an asphalt layer. A patched section consists of distressed roadway that is either cut out and/or directly filled with asphalt. Bad patches and potholes are jointly evaluated in the survey as they often require the same repairs and result from similar causes.

Rutting



Severity-- Low

Medium

High

Ruts are channelized depressions in the wheel tracks of a pavement surface. Most rutting is caused by a poor subgrade, excessive traffic load, and moisture.

Bleeding



Rated By Extent

Bleeding is an excess amount of liquid asphalt in the pavement mixture that has worked its way to the surface over time due to the repetitive stresses of traffic loads. Bleeding can appear as pavement with a smooth, black, shiny appearance that is sticky in hot, sunny weather. Bleeding can be caused by rich asphalt mixing, as well as the application of a heavy tack coat or excessive crack seal. When wet, a bleeding surface is as slippery as ice and can pose a serious safety hazard.

Drainage



Severity-- Medium



High



High

Uncontrolled water is extremely destructive to roadways. As previously stated in the Ten Commandments, the importance of having good drainage cannot be overemphasized. Ideally, water flows off of the crowned pavement surface, down a well graded shoulder, and into a clean ditching system where it can be carried away from the base.

In the more populated areas of Ticonderoga, good drainage can also include street curbs that guide water towards stormwater drains, underdrains, and catch basins.

Roughness

The overall smoothness of the road is characterized by roughness. A road receiving a poor roughness score could have sags, humps, potholes, or an uneven surface. Roughness is generally gauged by how observable the road's bumps and jolts are to passengers.

D. Roads by Rank

After the surveys were inputted into CAMP-RS, each road was categorized by its maintenance need as calculated by the software. The overall condition is summarized by the Priority

Condition Index--PCI. This is a numerical grading system from 0 to 94. Essentially the PCI is calculated by subtracting set values for various road distresses from 100. The amount of deducted points is determined by the extent and severity of the distresses as recorded by the surveys. So in short, the greater the distresses: the lower the PCI value.

Each section of road was assigned a priority value that can be compared with others to construct an order in which repairs should be tackled. Priority value takes into account Repair Category, PCI, Importance, Traffic, and Road Conditions.

E. Assign Repairs

In addition to the ranking values, the software recommends a repair category. The user can then select specific repairs to fit the needs of each individual road section.

Table 1. Category repair and description

| Category | Description |
|--------------------------|------------------------------------------------------------------------------------------------------------|
| Defer Maintenance | <i>Little to no road damage; no maintenance necessary</i> |
| Crack Repairs | <i>Road damage limited to low severity cracking</i> |
| Patching | <i>Routine maintenance required. Repairs range from pothole filling to replacing entire damaged areas.</i> |
| Drainage Work | <i>Any treatment related to reducing the effects of standing water or water flow on road integrity</i> |
| Surface Treatment | <i>Thin application of material above existing top coat to maintain smooth surface and repel water</i> |
| Overlay | <i>Thicker application of material; includes replacement of existing top coat layer</i> |

| | |
|-----------------------|--------------------------------------------------------------------------------------------------------------------|
| Rehabilitation | <i>Extensive recycling of base material; differs from reconstruction in project size</i> |
| Reconstruction | <i>Complete reinstallation of new pavement, including reconstruction and repair of drainage, base, and subbase</i> |

The bulk of the roads in town have gotten to the ‘Overlay’ stage or worse which means that sufficient repairs will cost significantly more. The cost for repairs for different pavement conditions can be seen below:

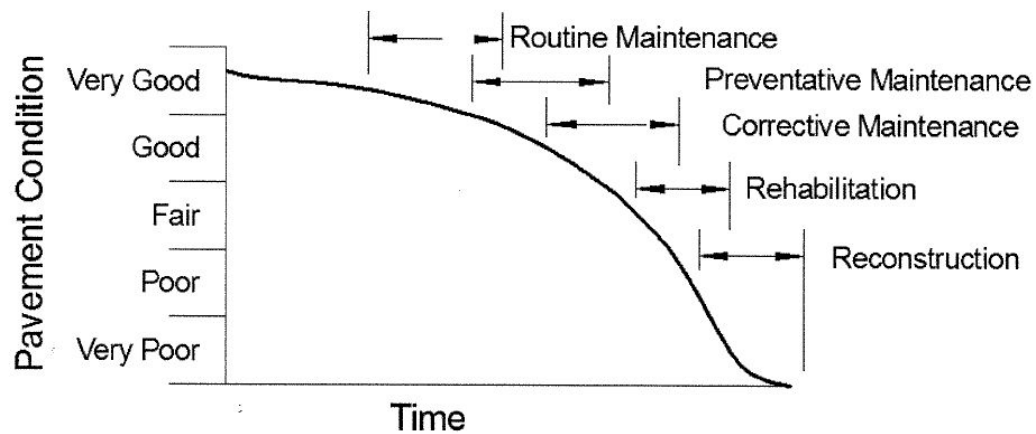


Figure 4. Pavement repair alternatives

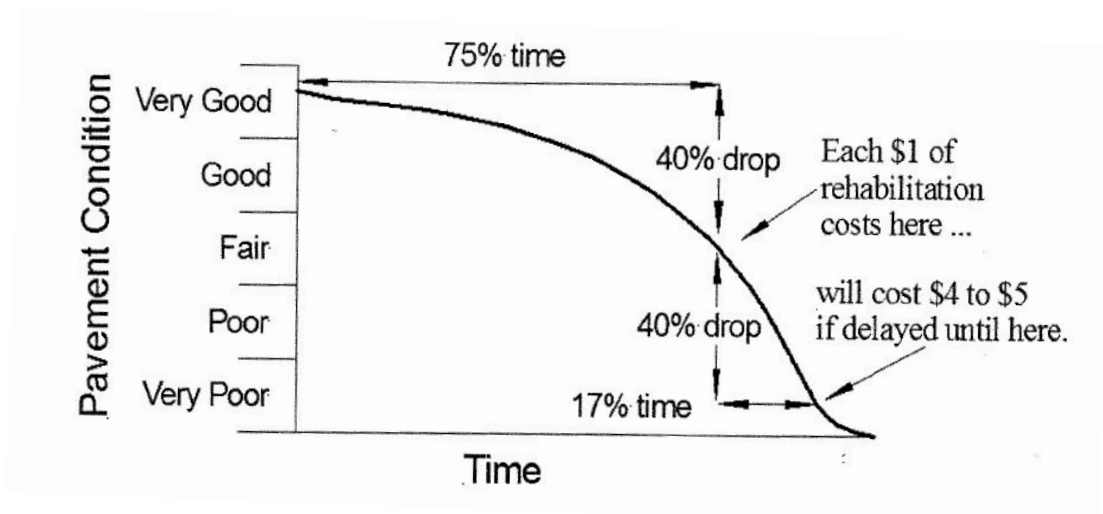


Figure 5. Pavement deterioration curve

After roads reach a certain point in their lifespan--called the acceptability index--condition deterioration tends to occur rapidly. Repairs on roads that are allowed to fall under the acceptability index may cost 4 to 5 times more than if it was repaired while still in good or fair condition. After falling below the acceptability index, roads may deteriorate as quickly as 7 times faster than its degradation rate while above index. The general goal of the program is to simultaneously maintain roads that are above the index while slowly raising poor quality roads into an acceptable condition.

III. Assessment Results

Upon evaluating all road conditions, it was determined that approximately 30% of roads in Ticonderoga are currently classified with a condition of Good or Very Good. The repairs done to these roads would typically fall under either the defer maintenance category or the crack repair category. Costs associated with these repairs would range between \$0 and \$2.00/ft². The roads that have already reached a fair condition or worse need much more expensive repairs--usually in excess of \$3 per. square foot--in order to improve their condition to the same quality. This is why it is imperative for municipalities to maintain and repair their good roads while simultaneously raising the poor roads to an acceptable condition.

However, approximately 70% of our roads are either in fair or poor condition: Both conditions are below the acceptability index for good quality roads. Some of these worse road conditions can present a serious safety hazard to all motorists. For example, excessive bleeding on roadways creates a slippery surface--akin to black ice--that poses a serious danger to motorists. Roads with deteriorating conditions quickly transform from being rough but passable in to posing a serious safety hazard. This past year, a portion of Warner Hill Road caved in underneath a plow truck. Serious harm could have come to those involved.

In order to remove these safety hazards, additional funding for road repairs is needed to ensure these issues are prevented in a timely manner.

The chart below provides a condition breakdown of all local roads in Ticonderoga by percentage and center-line miles, as well as a rough cost estimate for repair costs in each category.

Table 2. Overall local road assessment broken down by road conditions in Ticonderoga

| Road Condition | PCI Range (approx.) | Length (mi) | % of Town Roads | Cost (\$ / ft²) (approx.) | Typical Repair Category |
|-----------------------|----------------------------|--------------------|------------------------|---------------------------------------------|------------------------------------------------------------------|
| very good: | 94-92 | 4.96 | 10.6 | 0 to 1/2 | Defer Maintenance |
| good: | 92-88 | 9.65 | 20.7 | 0.5 to 2 | Crack Repairs, Patching, Surface Treatment, Drainage Work |
| fair: | 88-74 | 16.74 | 35.9 | 1.5 to 3 | Patching, Drainage Work, Overlay |
| poor: | 74-51 | 9.86 | 21.1 | 3 to 5 | Drainage Work, Overlay, Rehabilitation |
| very poor | 51-0 | 5.47 | 11.7 | 4 to 6 | Rehabilitation, Reconstruction |

IV. Recommendation

In the process of assigning a priority, the CAMP-RS program strictly follows the rule of “keeping good roads good” while correcting larger problems as funding allows. This algorithm is what drives the software decision process for road repair prioritization. In order to most effectively follow this plan, additional funding is needed.

The Superintendent--Sal Barnao--will use a copy of the attached CAMP--RS Report Spreadsheet to hand pick the most practical game plan going forward. This order would take into account factors a computer can’t consider; the work of the water and sewer departments, changes in road

condition that have occurred after the survey date, safety factors, seasonal changes in road use, and budget constraints.

V. Sources

Cornell Asset Management Program--Roads & Streets (CAMP-RS). Ithaca: Cornell Local Roads Program, 2016

Blades, Christopher and Edward Kearney. *Asphalt Paving Principles*. Ithaca: Cornell Local Roads Program, 2016

Muccin, Joseph. *Town of Mount Pleasant NY 2014 Final Report*. Mt. Pleasant: Mt. Pleasant Highway Department, 2014

Male, Paul. *Basics of a Good Road*. Ithaca: Cornell Local Roads Program, 2014

Appendix A

Ticonderoga Local Roads Assessment

| | | | | | | Ticonderoga CAMP-RS Section Report -- (Generated on 07/28/2016) | | | | | | | | | | | | | |
|----------|------------------|----------------|----------------|-------------|-----------|-----------------------------------------------------------------|--------------|-------|--------------|-----------------|-------------------|----------------------|-------------|-----------------|-----------------|----------|-------------------|-----------------------------------------------------------------------------------------|---------------------------------------------------------------------------------------------------------------------------------------------------------------------|
| RIN | Name | From | To | Start Miles | End Miles | Length | Width (feet) | Lanes | Surface Type | Shoulder Type | Traffic 1(Lo) - 5 | Importance 1(Lo) - 5 | Survey Date | Condition (PCI) | Repair Category | Priority | Attention Needed? | Road Notes | Sidewalk Notes |
| 105842-F | Lake George Ave. | Calkins Pl. | Highland St. | 0.51 | 0.7 | 0.19 | 32 | 2 | Asphalt | Curb - Concrete | 4 | 4 | 7/14/2016 | 52 | Drainage Work | 144 | | | Calkins Side--5ft*910ft, few 1" heaves, minimum cracking overall, fair condition \\Opposite Side--4ft*115ft, good condition |
| 105887-B | The Portage | Treadway St. | Defiance St. | 0.1 | 0.19 | 0.09 | 24 | 2 | Asphalt | Curb - Concrete | 4 | 4 | 7/5/2016 | 87 | Drainage Work | 132 | | Water is starting to cut under sidewalk | Treadway Side-- 4ft*full length, minimum cracking, good condition \\Opposite Side--4ft*full length, some parts poor surface condition, overall good |
| 105853-I | Montcalm Street | Cossey Street | Overlook Drive | 0.19 | 0.29 | 0.1 | 24 | 2 | Asphalt | Curb - Concrete | 4 | 5 | 6/29/2016 | 83 | Drainage Work | 132 | YES | Water Pooling and not reaching drain | Asphalt Side-- 4ft, full length, Completely overgrown, bad heaving and cracking, currently unusable |
| 105802-D | Burgoyne Rd. | Bennett Rd. | Village Ln. | 0.3 | 0.46 | 0.16 | 22 | 2 | Asphalt | Vegetation | 3 | 3 | 6/30/2016 | 77 | Drainage Work | 132 | YES | Water eating at road bed by Village intersection | Side--4ft*420ft, overgrown, very poor condition, sections missing, attention, 125ft completely in brush |
| 105805-A | Calkins Pl. | Champlain Ave. | Oak St. | 0 | 0.06 | 0.06 | 24 | 2 | Asphalt | Vegetation | 3 | 5 | 7/8/2016 | 75 | Drainage Work | 126 | YES | Vegetation beginning to cover road | Side--4ft*full length, overgrown, completely coverig sidewalk, poor drainage and surface deterioration |
| 105796 | Battery St. | The Portage | Dead End | 0 | 0.1 | 0.1 | 20 | 2 | Asphalt | Vegetation | 1 | 2 | 7/5/2016 | 87 | Drainage Work | 120 | YES | | Side--4ft*full length, extremely overgrown, large cracks, segements missing |
| 105887-D | The Portage | Battery St. | Carillon Rd. | 0.24 | 0.42 | 0.18 | 30 | 2 | Asphalt | Curb - Concrete | 4 | 4 | 7/5/2016 | 72 | Drainage Work | 120 | YES | Water is starting to cut under sidewalk | Battery Side--4ft*150ft, 2" heaving, minimum cracking, \\Opposite Side--4ft*full length, moderate cracking, poor cracking in front of Munsons |
| 105805-B | Calkins Pl. | Oak St. | Amherst Ave. | 0.05 | 0.11 | 0.06 | 24 | 2 | Asphalt | Vegetation | 3 | 5 | 7/8/2016 | 67 | Drainage Work | 120 | | Drains too High? | Oak Side--4ft, full length, minimal cracking, 1" heaves in some areas |
| 105899-B | Wayne Ave. | Montcalm St. | St. Clair St. | 0.16 | 0.28 | 0.12 | 28 | 2 | Asphalt | Curb - Apshalt | 3 | 3 | 6/29/2016 | 83 | Drainage Work | 114 | YES | Bottom of hill has water eroding the road edge | 4ft*full length, moderate cracking in few places, rough surface near top, fair coindition |
| 105827 | Grove Ave. | Hinds St. | St. Clair St. | 0 | 0.1 | 0.1 | 20 | 2 | Asphalt | Vegetation | 1 | 2 | 6/29/2016 | 80 | Drainage Work | 114 | | | Side--4ft*full length, minimal cracking, slight heaving" |
| 105899-A | Wayne Ave. | Schuyler St. | Montcalm St. | 0 | 0.16 | 0.16 | 28 | 2 | Asphalt | Curb - Concrete | 2 | 2 | 6/29/2016 | 69 | Drainage Work | 114 | YES | Water damage issues on roadway, poor edge pavement | Ti Pi Side-- 4.5ft*70ft, 5ft*85ft,4ft*200ft, fair condition \\ Opposite Side--4ft*full length, severe cracks, retaining wall falling into sidewalk, needs attention |
| 105838-A | John St. | Lord Howe St. | George St. | 0 | 0.1 | 0.1 | 28 | 2 | Asphalt | Vegetation | 1 | 2 | 6/30/2016 | 54 | Drainage Work | 114 | YES | Extremely poor side by Lord Howe, small section | Both Combined Side--4ft*500ft, severe cracking and patches, 5ft*500ft, perfect condition except for one 3"" heave, recommend grinding down |
| 105899-C | Wayne Ave. | St. Clair St. | Summit St. | 0.28 | 0.37 | 0.09 | 24 | 2 | Asphalt | Curb - Concrete | 2 | 2 | 6/29/2016 | 91 | Drainage Work | 108 | | road drainage in one area appears to be crumbling asphalt ditch, culvert may be cracked | Slate Side--4ft good condition, do not touch \\ Concrete Side--4ft*136ft, rough surface, minimum cracking, partially covered in sediment |

| | | | | | | | Ticonderoga CAMP-RS Section Report -- (Generated on 07/28/2016) | | | | | | | | | | | | |
|----------|--------------------|------------------|------------------|-------------|-----------|--------|-----------------------------------------------------------------|-------|--------------|-----------------|-------------------|----------------------|-------------|-----------------|-----------------|----------|-------------------|--------------------------------------------------------------------------------------------------|---------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|
| | | | | | | | | | | | | | | | | | | | |
| RIN | Name | From | To | Start Miles | End Miles | Length | Width (feet) | Lanes | Surface Type | Shoulder Type | Traffic 1(Lo) - 5 | Importance 1(Lo) - 5 | Survey Date | Condition (PCI) | Repair Category | Priority | Attention Needed? | Road Notes | Sidewalk Notes |
| 105856-D | Mt. Hope Ave. | Mohawk Tr. | Sunset St. | 0.26 | 0.41 | 0.15 | 22 | 2 | Asphalt | Vegatation | 2 | 2 | 6/30/2016 | 87 | Drainage Work | 108 | | Water bypasses two drains on roadway | Side--4ft*325ft, minimum cracking, upper 30ft has poor surface |
| 105856-A | Mt. Hope Ave. | Wiley St. | Morehouse Dr. | 0 | 0.1 | 0.1 | 24 | 2 | Asphalt | Curb - Concrete | 2 | 2 | 6/30/2016 | 86 | Drainage Work | 108 | | Standing water on corner of road | Side--4ft*570ft, minimum cracking, good condition |
| 105881-F | St. Clair St. | Grove Ave. | Wicker St. | 0.3 | 0.38 | 0.08 | 24 | 2 | Asphalt | Curb - Concrete | 3 | 3 | 6/29/2016 | 72 | Drainage Work | 108 | YES | | Side-- 4ft*full length, areas around driveway need attention |
| 105864 | Pearl St. | Lord Howe St. | Dead End | 0 | 0.19 | 0.19 | 20 | 2 | Asphalt | Vegatation | 1 | 2 | 6/30/2016 | 72 | Drainage Work | 108 | | | No Sidewalk |
| 105834 | Hinds St. | Wicker St. | Dead End | 0 | 0.15 | 0.15 | 20 | 2 | Asphalt | Curb - Concrete | 1 | 2 | 6/29/2016 | 67 | Drainage Work | 108 | YES | Dead end floods, street damage from heavy trucks in 2015 | Side-- 4ft*full length, minimal cracking, severe corner break by traffic cones |
| 105813 | Colonial St. | The Portage | Dead End | 0 | 0.19 | 0.19 | 20 | 2 | Asphalt | Vegatation | 1 | 2 | 7/7/2016 | 91 | Drainage Work | 102 | | | No Sidewalk |
| 105847 | Lonerган Ln. | Burgoyne Rd. | Dead End | 0 | 0.05 | 0.05 | 12 | 2 | Asphalt | Vegatation | 1 | 2 | 6/30/2016 | 86 | Drainage Work | 102 | | | No Sidewalk |
| 105901 | Woody Ln | Treadway St. | Dead End | 0 | 0.1 | 0.1 | 28 | 2 | Asphalt | Curb - Concrete | 1 | 2 | 7/5/2016 | 62 | Drainage Work | 102 | | | Rock Side--4ft*425ft, 100ft in poor condition, moderate heaving, \\' timber Side-- 4ft*full length, moderate cracking, 2'" heaves, few corner breaks |
| 105884 | Stoughton Dr. | Baldwin Dr. | Dead End | 0 | 0.3 | 0.3 | 16 | 2 | Asphalt | Vegatation | 2 | 2 | 7/18/2016 | 48 | Drainage Work | 102 | | sediment buildup on road, no drains, 1" ruts, poor sloping of edges | No Sidewalk |
| 105841 | Killicut Mtn. Rd | County Rt. 56 | Dead End | 0 | 0.87 | 0.87 | 20 | 2 | Asphalt | Vegatation | 1 | 1 | 7/19/2016 | 43 | Drainage Work | 102 | YES | road only extends to turn around sign, 1 section of terrible edge cracking, road being destroyed | No Sidewalk |
| 105897-B | Warner Hill Rd. | Pavement Change | Town Line | 2.02 | 4.89 | 2.87 | 20 | 2 | Asphalt | Vegatation | 1 | 1 | 7/19/2016 | 40 | Reconstruction | 102 | | Will be paved Summer and Fall of 2016 | No Sidewalk |
| 105859 | Oak St. | Calkins Pl. | Dead End | 0 | 0.05 | 0.05 | 16 | 2 | Asphalt | Vegatation | 1 | 1 | 7/14/2016 | 91 | Drainage Work | 96 | | | No Sidewalk |
| 105824 | Frasier Bridge Dr. | Burgoyne Rd. | Dead End | 0 | 0.05 | 0.05 | 38 | 2 | Asphalt | Vegatation | 1 | 2 | 6/30/2016 | 76 | Drainage Work | 96 | | Crumbling at Burgoyne intersection | No Sidewalk |
| 257731-C | Birnbaum Rd. | Closed Road | Vinyard Rd. | 0.86 | 1.16 | 0.3 | 32 | 2 | Unpaved | Vegatation | 1 | 1 | 7/19/2016 | 56 | Drainage Work | 95 | | Services one House | No Sidewalk |
| 105798 | Bennett Rd. | Burgoyne Rd. | Dead End | 0 | 0.1 | 0.1 | 16 | 2 | Asphalt | Vegatation | 1 | 2 | 6/30/2016 | 74 | Drainage Work | 90 | | | No Sidewalk |
| 105890-A | Tin Pan Alley | The Portage | Water St. | 0 | 0.15 | 0.15 | 20 | 4 | Asphalt | Vegatation | 1 | 2 | 7/14/2016 | 71 | Drainage Work | 90 | | | No Sidewalk |
| 105814 | Commerce Dr. | Rt. 74 | Dead End | 0 | 0.19 | 0.19 | 20 | 2 | Asphalt | Gravel | 1 | 2 | 7/19/2016 | 67 | Drainage Work | 90 | | | No Sidewalk |
| 105843 | Lead Hill Rd. | Rt. 74 | Dead End | 0 | 0.22 | 0.22 | 18 | 2 | Asphalt | Vegatation | 1 | 1 | 7/19/2016 | 54 | Drainage Work | 90 | YES | 4in longitudinal cracks needs attention | No Sidewalk |
| 105896 | Village Ln. | Burgoyne Rd. | Dead End | 0 | 0.2 | 0.2 | 22 | 2 | Asphalt | Vegatation | 1 | 2 | 6/30/2016 | 39 | Drainage Work | 90 | | | No Sidewalk |
| 105853-F | Montcalm Street | Tower Avenue | Champlain Ave. | 0.61 | 0.67 | 0.06 | 56 | 2 | Asphalt | Curb - Concrete | 5 | 5 | 6/29/2016 | 90 | Crack Repairs | 84 | | | Tower Side--6ft*full length, good condition \\'Opposite Side--6ft*full length, minimum surface deterioration |
| 105853-E | Montcalm Street | Champlain Avenue | Carnegie Place | 0.67 | 0.78 | 0.11 | 50 | 2 | Asphalt | Curb - Concrete | 5 | 5 | 6/29/2016 | 90 | Crack Repairs | 84 | | | Carnegie Side--5ft*90ft, minimum heaving, 8ft*500ft, good overall, few spots need patching \\' Opposite Side-- 9ft*90ft, moderate cracking, 8ft*130ft, moderate joint condition, 7ft*80ft, minimum cracking, 5ft*80ft, good condition |
| 105853-D | Montcalm Street | Carnegie Place | Lake George Ave. | 0.78 | 0.84 | 0.06 | 36 | 2 | Asphalt | Curb - Concrete | 5 | 5 | 6/29/2016 | 90 | Crack Repairs | 84 | | | Carnegie Side--5ft*full length, good condition \\'Schuyler Side--avg 5ft*full length, good condition, minimum heaving |

| | | | | | | Ticonderoga CAMP-RS Section Report -- (Generated on 07/28/2016) | | | | | | | | | | | | | |
|----------|-----------------|-----------------|-------------------|----------------|--------------|-----------------------------------------------------------------|-----------------|-------|-----------------|------------------|----------------------|-------------------------|----------------|--------------------|-----------------|----------|----------------------|----------------------------------------------------------|-------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|
| RIN | Name | From | To | Start Miles | End Miles | Length | Width (feet) | Lanes | Surface Type | Shoulder Type | Traffic 1(Lo) - 5 | Importance 1(Lo) - 5 | Survey Date | Condition (PCI) | Repair Category | Priority | Attention Needed? | Road Notes | Sidewalk Notes |
| 105853-B | Montcalm Street | Wiley Street | Wayne Avenue | 0.91 | 1.1 | 0.19 | 38 | 2 | Asphalt | Curb - Concrete | 5 | 5 | 6/29/2016 | 88 | Crack Repairs | 84 | | | Schuyler Side--6ft*full length, minor edge breaks, moderate surface deterioration \\Wiley Side--6ft*75ft, 5ft*rest of length, good condition, minimum cracking |
| 105853-A | Montcalm Street | Wayne Avenue | Lord Howe | 1.1 | 1.3 | 0.2 | 38 | 2 | Asphalt | Curb - Concrete | 5 | 5 | 6/29/2016 | 88 | Crack Repairs | 84 | | | Lord Howe Side-- 5ft*450ft, 4ft*180ft, poor surface condition by hot biscuit \\Opposite Side--5ft*full length, minimum cracking" |
| 105811-E | Champlain Ave. | The Portage | Calkins Pl. | 0.26 | 0.59 | 0.33 | 24 | 2 | Asphalt | Curb - Concrete | 5 | 5 | 7/7/2016 | 88 | Crack Repairs | 84 | YES | | Armory Side-- 5ft*full length, minimum cracking, overall good condition \\Portage Side--4ft*975ft, 400ft needs attention, poor surface deterioration, heaving, large cracking \\EMS triangle park Side--6ft*185ft, moderate cracking, needs attention |
| 105811-C | Champlain Ave. | Algonkin St. | Father Jogues St. | 0.13 | 0.21 | 0.08 | 32 | 2 | Asphalt | Curb - Concrete | 5 | 5 | 7/7/2016 | 88 | Crack Repairs | 84 | | | Father Jogues Side--4ft*full length, very good condition \\Opposite Side--5.5ft*full length, vary minimal cracking, very good condition |
| 105804 | Caldwell Cir. | Baldwin Rd. | Baldwin Rd. | 0 | 0.07 | 0.07 | 16 | 2 | Asphalt | Vegetation | 1 | 1 | 7/18/2016 | 67 | Drainage Work | 84 | | Alligator cracking starting from the center green | No Sidewalk |
| 105877 | Shattuck Rd. | Bull Rock Rd. | Dead End | 0 | 0.34 | 0.34 | 12 | 2 | Unpaved | Vegetation | 1 | 1 | 7/19/2016 | 62 | Drainage Work | 80 | | good rd conditon as of 7/2016 | No Sidewalk |
| 105887-C | The Portage | Defiance St. | Battery St. | 0.19 | 0.24 | 0.05 | 30 | 2 | Asphalt | Vegetation | 4 | 4 | 7/5/2016 | 92 | Crack Repairs | 76 | | | Battery Side--5ft*full length, minimum surface deterioration, good condition \\Opposite Side--4ft*full length, good condition |
| 105792-C | Alexandria Ave. | Water St. | Lake George Ave. | 0.15 | 0.24 | 0.09 | 40 | 2 | Asphalt | Paved - Aspahlt | 4 | 4 | 7/14/2016 | 90 | Crack Repairs | 76 | YES | | Lake George Side--4ft*full length, <1"" heaves, 1 section missing, minimal cracking, fair condition \\Opposite Side--4ft*215ft, severe surface deterioration, moderate cracking |
| 105846-E | Lord Howe St. | George St. | Montcalm St. | 0.83 | 0.91 | 0.08 | 30 | 2 | Asphalt | Vegetation | 4 | 4 | 6/30/2016 | 88 | Crack Repairs | 76 | | | Side--4ft*full length, good condition, minimum cracking |
| 105842-A | Lake George Ave | Montcalm St. | Algonkin St. | 0 | 0.04 | 0.04 | 40 | 2 | Asphalt | Curb - Concrete | 4 | 4 | 7/14/2016 | 88 | Crack Repairs | 76 | | | Algonkin Side--6ft*full length, very good condition \\Opposite Side--5ft*55ft, good condition |
| 105887-H | The Portage | Alexandria Ave. | Abercrombie St. | 0.71 | 0.73 | 0.02 | 28 | 2 | Asphalt | Vegetation | 4 | 4 | 7/5/2016 | 86 | Crack Repairs | 76 | | | Side--4ft*full length, minimum cracking good condition |
| 105811-G | Champlain Ave. | Carillon Rd. | Highland St. | 0.68 | 0.74 | 0.06 | 24 | 2 | Asphalt | Curb - Concrete | 4 | 4 | 7/7/2016 | 86 | Crack Repairs | 76 | | | Side--4ft*full length, 1"" heaves, minimal cracking, fair condition |
| 105853-J | Montcalm Street | Cossey St. | Rt 74 & 22 | 0 | 0.19 | 0.19 | 22 | 2 | Asphalt | Curb - Concrete | 4 | 5 | 6/29/2016 | 84 | Crack Repairs | 76 | YES | Curb on bridge is falling out/gone, needs to be replaced | Bridge Side-- 4ft*full length both sides, heaving as bad as 3"", severe edge cracking, sections of granite curb coming out |
| 105853-H | Montcalm Street | Overlook Drive | Cannon Ball | 0.29 | 0.44 | 0.15 | 32 | 2 | Asphalt | Vegetation | 4 | 5 | 6/29/2016 | 82 | Patching | 76 | YES | | Asphalt Side-- 4ft*235ft, bad heaving and cracking |

| | | | | | | Ticonderoga CAMP-RS Section Report -- (Generated on 07/28/2016) | | | | | | | | | | | | | |
|-----------|------------------|-----------------|--------------------|-------------|-----------|-----------------------------------------------------------------|--------------|-------|--------------|------------------|-------------------|----------------------|-------------|-----------------|-----------------|----------|-------------------|--------------------------------------|----------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|
| RIN | Name | From | To | Start Miles | End Miles | Length | Width (feet) | Lanes | Surface Type | Shoulder Type | Traffic 1(Lo) - 5 | Importance 1(Lo) - 5 | Survey Date | Condition (PCI) | Repair Category | Priority | Attention Needed? | Road Notes | Sidewalk Notes |
| 105830 | Hayford Rd. | New Hague Rd. | Dead End | 0 | 0.28 | 0.28 | 16 | 2 | Unpaved | Vegatation | 1 | 1 | 7/19/2016 | 83 | Drainage Work | 75 | | | No Sidewalk |
| 105890-B | Tin Pan Alley | Water St. | Dead End | 0.15 | 0.2 | 0.05 | 12 | 2 | Unpaved | Vegatation | 1 | 1 | 7/19/2016 | 81 | Drainage Work | 75 | | | No Sidewalk |
| 105900-C | Wiley St. | Uncas Dr. | Grace Ave. | 0.14 | 0.21 | 0.07 | 24 | 2 | Asphalt | Curb - Concrete | 4 | 3 | 7/19/2016 | 92 | Crack Repairs | 72 | YES | | Side--4ft, full length, poor surface in 1 to 2 areas, needs attention, few large chunks missig from sidewalk |
| 105900-B | Wiley St. | Mt. Hope Ave. | Uncas Dr. | 0.09 | 0.14 | 0.05 | 24 | 2 | Asphalt | Curb - Concrete | 4 | 3 | 6/30/2016 | 92 | Crack Repairs | 72 | YES | | Uncas Side-- 5ft*full length, very good condition \\ Opposite Side--4ft*full length,joint break on upper side, extends beyond uncas intrsection 20ft |
| 105900-A | Wiley St. | Moncalm St. | Mt. Hope Ave. | 0 | 0.09 | 0.09 | 28 | 2 | Asphalt | Vegatation | 4 | 3 | 6/30/2016 | 92 | Crack Repairs | 72 | | | Side--5ft*full length, great condition, sidewalk does switch side but runs full length |
| 105887-G | The Portage | Crown Hts. | Alexandria Ave. | 0.64 | 0.71 | 0.07 | 34 | 2 | Asphalt | Curb - Concrete | 4 | 3 | 7/5/2016 | 92 | Crack Repairs | 72 | | | Side--4ft*full length, perfect condition |
| 105870-B | Race Track Rd. | Old Chilson Rd. | Rt. 74 | 0.37 | 0.74 | 0.37 | 20 | 2 | Asphalt | Vegatation | 3 | 4 | 7/19/2016 | 90 | Crack Repairs | 72 | | | No Sidewalk |
| 105792-B | Alexandria Ave. | Champlain Ave. | Water St. | 0.05 | 0.15 | 0.1 | 34 | 2 | Asphalt | Paved - Aspahlit | 3 | 4 | 7/14/2016 | 90 | Crack Repairs | 72 | YES | | Champlain Side--4ft*full length, some sections gone, large potholes \\ Water Side--4ft*full length, good condition |
| 105870-A | Race Track Rd. | Wicker St. | Old Chilson Rd. | 0 | 0.37 | 0.37 | 20 | 2 | Asphalt | Vegatation | 3 | 4 | 7/19/2016 | 88 | Crack Repairs | 72 | | Recently repaired patch | No Sidewalk |
| 105802-G | Burgoyne Rd. | Heather Hts. | Frasier Bridge Dr. | 0.72 | 0.79 | 0.07 | 22 | 2 | Asphalt | Curb - Concrete | 3 | 4 | 6/30/2016 | 86 | Patching | 72 | YES | sewer floods over road during storms | Frazier Side-- 5ft*100ft, edge breaks, moderate cracks, 4ft*270ft, some heaving, decent condition \\ Opposite Side--4ft*115ft, very overgrown |
| 105805-C | Calkins Pl. | Amherst Ave. | Newton St. | 0.11 | 0.16 | 0.05 | 42 | 2 | Asphalt | Curb - Concrete | 3 | 5 | 7/14/2016 | 85 | Patching | 72 | | | Oak Side--4ft,full length, good surface, few 1"" heaves, good overall \\ Opposite Side--4ft*full length, minimum cracking, good condition overall" |
| 105842-G | Lake George Ave. | Highland St. | Alexandria Ave. | 0.7 | 0.9 | 0.2 | 30 | 2 | Asphalt | Curb - Concrete | 4 | 4 | 7/14/2016 | 82 | Patching | 72 | | | Side--4t, full length, minimum cracking, fair condition |
| 105842-D | Lake George Ave. | Iroquois St. | Stanton St. | 0.14 | 0.31 | 0.17 | 32 | 2 | Asphalt | Curb - Concrete | 4 | 4 | 7/19/2016 | 79 | Patching | 72 | YES | | Iroquois Side--5ft*380ft, 2"" heaves, sections tilting, some sections gone, 4ft*435ft, good condition \\ Opposite Side--5ft*full length, sections by driveway with severe surface deterioration |
| 105-887-L | The Portage | Colonial St. | Water St. | 0.93 | 1.03 | 0.1 | 24 | 2 | Asphalt | Vegatation | 3 | 3 | 7/6/2016 | 92 | Crack Repairs | 68 | YES | Possible drainage issues | Side--4ft*full length, good condition |
| 105887-K | The Portage | Pinnacle St. | Colonial St | 0.87 | 0.93 | 0.06 | 24 | 2 | Asphalt | Vegatation | 3 | 3 | 7/6/2016 | 92 | Crack Repairs | 68 | | | Side--4ft*full length, good condition |
| 105887-J | The Portage | Tin Pan Alley | Pinnacle St. | 0.85 | 0.87 | 0.02 | 24 | 2 | Asphalt | Curb - Concrete | 3 | 3 | 7/5/2016 | 92 | Crack Repairs | 68 | | | Side--4ft*full length, very good condition |
| 105875-B | Schuyler St. | Wayne Ave. | John St. | 0.15 | 0.25 | 0.1 | 22 | 2 | Asphalt | Vegatation | 3 | 3 | 6/30/2016 | 92 | Crack Repairs | 68 | | | Side--4ft*full length, good overall |
| 105875-A | Schuyler St. | Montcalm St. | Wayne Ave. | 0 | 0.15 | 0.15 | 28 | 2 | Asphalt | Vegatation | 3 | 3 | 6/30/2016 | 92 | Crack Repairs | 68 | | | Side-- 4ft*1450ft, minimal heaving, great condition overall |
| 105802-C | Burgoyne Rd. | Mt. Hope Ave. | Bennett Rd. | 0.23 | 0.3 | 0.07 | 22 | 2 | Asphalt | Vegatation | 3 | 3 | 6/30/2016 | 90 | Crack Repairs | 68 | | | No Sidewalk |
| 105863 | Patriot Pass | Mt. Hope Ave. | Grace Ave. | 0 | 0.13 | 0.13 | 18 | 2 | Asphalt | Vegatation | 3 | 3 | 6/30/2016 | 90 | Crack Repairs | 68 | | | No Sidewalk |
| 105887-I | The Portage | Abercrombie St. | Tin Pan Alley | 0.73 | 0.85 | 0.12 | 24 | 2 | Asphalt | Vegatation | 3 | 3 | 7/5/2016 | 88 | Crack Repairs | 68 | | | Side--4ft* full length, good condition |
| 105802-B | Burgoyne Rd. | Park Ave. | Mt. Hope Ave. | 0.04 | 0.23 | 0.19 | 22 | 2 | Asphalt | Vegatation | 3 | 3 | 6/30/2016 | 86 | Crack Repairs | 68 | | | No Sidewalk |

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|----------|-----------------|-------------------|------------------|-------------|-----------|--------|-----------------------------------------------------------------|-------|--------------|-----------------|-------------------|----------------------|-------------|-----------------|--------------------|----------|-------------------|---------------------------------------------------------|-------------------------------------------------------------------------------------------------------------------------------------------------------|
| | | | | | | | | | | | | | | | | | | | |
| RIN | Name | From | To | Start Miles | End Miles | Length | Width (feet) | Lanes | Surface Type | Shoulder Type | Traffic 1(Lo) - 5 | Importance 1(Lo) - 5 | Survey Date | Condition (PCI) | Repair Category | Priority | Attention Needed? | Road Notes | Sidewalk Notes |
| 105881-D | St. Clair St. | Second St. | Third St. | 0.15 | 0.2 | 0.05 | 30 | 2 | Asphalt | Curb - Concrete | 2 | 3 | 6/29/2016 | 92 | Crack Repairs | 64 | | | Second Side-- 4ft,*full length, minimal cracking, sediment pooling up to ~1" |
| 105811-A | Champlain Ave. | McCormick St. | Montcalm St. | 0 | 0.04 | 0.04 | 50 | 2 | Asphalt | Curb - Concrete | 2 | 3 | 7/7/2016 | 92 | Crack Repairs | 64 | | | Abuchaun Side--6ft*full length, good condition \\Opposite Side--6ft*full length, great condition |
| 105881-A | St. Clair St. | Grace Ave | Park Ave | 0 | 0.05 | 0.05 | 28 | 2 | Asphalt | Vegatation | 2 | 3 | 6/29/2016 | 90 | Crack Repairs | 64 | | | Park Side-- 4ft, full length, good condition |
| 105881-B | St. Clair St. | Park Ave | Wayne Ave | 0.05 | 0.1 | 0.05 | 28 | 2 | Asphalt | Vegatation | 2 | 3 | 6/29/2016 | 89 | Patching | 64 | | | Side-- 4ft*full length, good condition |
| 105802-J | Burgoyne Rd. | Rt. 74 & 22 | Rt. 9N & 22 | 1.38 | 2.24 | 0.86 | 18 | 2 | Asphalt | Vegatation | 3 | 3 | 6/30/2016 | 85 | Crack Repairs | 64 | | | No Sidewalk |
| 105829-B | Hawkeye | Musketball Run | Cannonball | 0.05 | 0.09 | 0.04 | 22 | 2 | Asphalt | Vegatation | 2 | 4 | 7/5/2016 | 83 | Patching | 64 | | | No Sidewalk |
| 105802-E | Burgoyne Rd. | Village Ln. | Lonergan Ln. | 0.46 | 0.62 | 0.16 | 22 | 2 | Asphalt | Vegatation | 3 | 3 | 6/30/2016 | 83 | Patching | 64 | | | Side--4ft*230ft, come owner trying to grow grass over sidewalk, 120ft in poor condition |
| 105853-C | Montcalm Street | Lake George Ave. | Wiley Street | 0.84 | 0.91 | 0.07 | 40 | 2 | Asphalt | Curb - Concrete | 5 | 5 | 6/29/2016 | 88 | Overlay | 63 | | | Wiley Side--6ft*full length, good condition \\ Lake George Side-- 5ft*full length, poor joint sealing, joint pieces coming out |
| 105887-A | The Portage | Champlain Ave. | Treadway St. | 0 | 0.1 | 0.1 | 28 | 2 | Asphalt | Curb - Concrete | 4 | 4 | 7/5/2016 | 76 | Overlay | 63 | | | Side-- 5ft*full length, perect condition |
| 105888-B | Third Ave. | Summit St. | Dead End | 0.1 | 0.33 | 0.23 | 20 | 2 | Asphalt | Vegatation | 2 | 2 | 6/29/2016 | 92 | Crack Repairs | 60 | | | Side--4ft*95ft, 1 4'" sag but overall surface still in good condition |
| 105807 | Carillon Rd. | The Portage | Champlain Ave. | 0 | 0.12 | 0.12 | 22 | 2 | Asphalt | Curb - Concrete | 2 | 2 | 7/7/2016 | 92 | Crack Repairs | 60 | | | Side--4ft*full length, two 2'" heaves, minimum edge cracking, fair condition |
| 105795 | Aviation Rd. | Shanahan Rd. | Dead End | 0 | 0.05 | 0.05 | 16 | 2 | Asphalt | Vegatation | 1 | 3 | 7/19/2016 | 92 | Crack Repairs | 60 | | | No Sidewalk |
| 105856-F | Mt. Hope Ave. | Burgoyne Ave | Rt 22/74 | 0.51 | 0.67 | 0.16 | 22 | 2 | Asphalt | Vegatation | 2 | 2 | 6/30/2016 | 90 | Crack Repairs | 60 | | | No Sidewalk |
| 105856-E | Mt. Hope Ave. | Sunset St. | Burgoyne Rd. | 0.41 | 0.51 | 0.1 | 22 | 2 | Asphalt | Vegatation | 2 | 2 | 6/30/2016 | 90 | Crack Repairs | 60 | | | No Sidewalk |
| 105895 | Veterans Rd. | Rt. 9N / 22 | Dead End | 0 | 0.95 | 0.95 | 20 | 2 | Asphalt | Vegatation | 2 | 2 | 7/19/2016 | 90 | Crack Repairs | 60 | | | No Sidewalk |
| 105871 | River Rd. | Montcalm St. | Rt. 22 | 0 | 0.5 | 0.5 | 24 | 2 | Asphalt | Vegatation | 2 | 2 | 7/1/2016 | 90 | Crack Repairs | 60 | | | No Sidewalk |
| 105862-C | Park Ave. | Sunset St. | Burgoyne Ave. | 0.34 | 0.54 | 0.2 | 20 | 2 | Asphalt | Vegatation | 2 | 2 | 6/29/2016 | 88 | Crack Repairs | 60 | | | No Sidewalk |
| 105856-C | Mt. Hope Ave. | Patriot Pass | Mohawk Tr. | 0.2 | 0.26 | 0.06 | 23 | 2 | Asphalt | Vegatation | 2 | 2 | 6/30/2016 | 88 | Crack Repairs | 60 | | | No Sidewalk |
| 105837-B | Iroquois St. | Holcomb Ave. | Lake George Ave. | 0.05 | 0.1 | 0.05 | 26 | 2 | Asphalt | Curb - Concrete | 2 | 2 | 7/7/2016 | 86 | Crack Repairs | 60 | | | Holcomb Side--4ft*full length, good condition \\Opposite Side--4ft*full length, great condition |
| 105833-A | Highland St. | The Portage | Champlain Ave. | 0 | 0.12 | 0.12 | 22 | 2 | Asphalt | Vegatation | 2 | 2 | 7/7/2016 | 86 | Patching | 60 | YES? | Pooling of water on properties, road drainage an issue? | Side--4ft*full length, minimum cracking , minimal heaving, great overall |
| 105853-G | Montcalm Street | Cannon Ball | Tower Avenue | 0.44 | 0.61 | 0.17 | 24 | 2 | Asphalt | Vegatation | 5 | 5 | 6/29/2016 | 80 | Overlay | 60 | | | Side--5ft*120ft, perfect condition |
| 105811-D | Champlain Ave | Father Jogues St. | The Portage | 0.21 | 0.26 | 0.05 | 34 | 2 | Asphalt | Curb - Concrete | 5 | 5 | 7/7/2016 | 79 | Surface Treatments | 60 | | | Father Jogues Side--4ft*full length, good condition \\Portage Side--4ft*full length, fair condition |
| 105794-A | Amherst Ave. | Father Jogues St. | Iroquois St | 0 | 0.05 | 0.05 | 26 | 2 | Asphalt | Curb - Concrete | 3 | 5 | 7/7/2016 | 74 | Overlay | 60 | YES | | Church Side--4ft*435ft, good condition \\Opposite Side-- 4ft* full length, severe cracking, small heaves, severe corner breaks, high priority area |
| 105846-C | Lord Howe St. | Schuyler St. | John St. | 0.64 | 0.74 | 0.1 | 20 | 2 | Asphalt | Curb - Concrete | 4 | 4 | 6/30/2016 | 72 | Overlay | 60 | YES | | Side-- 4ft*full length, large amount of cracking, significant heaving, poor cracking over driveways |

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|----------|------------------|-------------------|------------------|----------------|--------------|-----------------------------------------------------------------|-----------------|-------|-----------------|------------------|----------------------|-------------------------|----------------|--------------------|--------------------|----------|----------------------|----------------------------------------------|------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|
| RIN | Name | From | To | Start Miles | End Miles | Length | Width (feet) | Lanes | Surface Type | Shoulder Type | Traffic 1(Lo) - 5 | Importance 1(Lo) - 5 | Survey Date | Condition (PCI) | Repair Category | Priority | Attention Needed? | Road Notes | Sidewalk Notes |
| 105794-B | Amherst Ave. | Iroquois St. | Calkins Pl. | 0.05 | 0.38 | 0.33 | 26 | 2 | Asphalt | Curb - Concrete | 3 | 5 | 7/7/2016 | 70 | Overlay | 60 | | | St Mary's Side--4ft*full length, poor surface deterioration for 25ft, moderate cracking \\ high school Side--4ft*full length, portions of sidewalk slanted, minimal cracking, 1"" heaves |
| 105875-C | Schuyler St. | John St. | Lord Howe St. | 0.25 | 0.39 | 0.14 | 22 | 2 | Asphalt | Earth | 3 | 3 | 6/30/2016 | 84 | Overlay | 57 | YES | Pavement does not extend to sidewalk | Side--54ft*full length, moderate corner breaks, low cracking, sometimes becomes overgrown |
| 105846-D | Lord Howe St. | John St. | George St. | 0.74 | 0.83 | 0.09 | 20 | 2 | Asphalt | Vegetation | 4 | 4 | 6/30/2016 | 65 | Overlay | 57 | | | Side--4ft*full length, good condition |
| 105846-A | Lord Howe St. | Alexandria Ave. | Pearl St. | 0 | 0.16 | 0.16 | 20 | 2 | Asphalt | Vegetation | 4 | 4 | 6/30/2016 | 59 | Overlay | 57 | YES | extremely poor rutting, needs attention | Side--4ft*645ft, extremely overgrown, sections near driveways have grass cover |
| 105878-B | Shanahan Rd. | Shore Airport Rd. | Aviation Rd. | 0.45 | 0.69 | 0.24 | 18 | 2 | Asphalt | Vegetation | 1 | 2 | 7/18/2016 | 92 | Crack Repairs | 56 | | | No Sidewalk |
| 105851 | Myers St. | Cossey St. | Dead End | 0 | 0.13 | 0.13 | 16 | 2 | Asphalt | Vegetation | 1 | 2 | 7/5/2016 | 92 | Crack Repairs | 56 | | | No Sidewalk |
| 105833-C | Highland St. | Ell St. | Lake George Ave. | 0.24 | 0.35 | 0.11 | 18 | 2 | Asphalt | Curb - Concrete | 1 | 2 | 7/7/2016 | 90 | Crack Repairs | 56 | | | Side--4ft, full length, minimum cracking, good condition |
| 105882 | Stanton St. | Lake George Ave. | Dead End | 0 | 0.06 | 0.06 | 20 | 2 | Asphalt | Curb - Concrete | 1 | 2 | 7/14/2016 | 90 | Crack Repairs | 56 | | | Side--5ft, full length, good condition |
| 105812 | Charboneau Rd. | NYS 9N / 22 | Delano Rd. | 0 | 0.92 | 0.92 | 18 | 2 | Asphalt | Vegetation | 1 | 2 | 7/19/2016 | 90 | Crack Repairs | 56 | | | No Sidewalk |
| 105854 | Morehouse Dr. | Grace Ave. | Mt. Hope Ave. | 0 | 0.08 | 0.08 | 26 | 2 | Asphalt | Curb - Concrete | 1 | 2 | 6/30/2016 | 89 | Patching | 56 | YES | | Side--4ft*full length, surface wearing out, few moderate cracks, lower half needs attention |
| 105826-D | Grace Ave | Dead End | Mohawk Trail | 0.13 | 0.3 | 0.17 | 22 | 2 | Asphalt | Vegetation | 1 | 2 | 6/30/2016 | 88 | Crack Repairs | 56 | | | No Sidewalk |
| 105865 | Pinnacle St. | The Portage | Dead End | 0 | 0.18 | 0.18 | 20 | 2 | Asphalt | Vegetation | 1 | 2 | 7/7/2016 | 88 | Crack Repairs | 56 | | | No Sidewalk |
| 105857 | Newton St. | Calkins Pl. | Dead End | 0 | 0.05 | 0.05 | 12 | 1 | Asphalt | Vegetation | 1 | 2 | 7/14/2016 | 88 | Crack Repairs | 56 | | | No Sidewalk |
| 105826-E | Grace Ave | Mohawk Tr. | Sunset St. | 0.3 | 0.48 | 0.18 | 20 | 2 | Asphalt | Vegetation | 1 | 2 | 6/30/2016 | 86 | Patching | 56 | | Small sediment collection on edge of roadway | No Sidewalk |
| 105837-A | Iroquois St. | Amherst Ave. | Holcomb Ave. | 0 | 0.05 | 0.05 | 30 | 2 | Asphalt | Curb - Concrete | 2 | 2 | 7/7/2016 | 83 | Patching | 56 | YES? | | Holcomb Side--4ft*full length, good condition \\Opposite 4ft, full length, good condition overall, 10ft has moderate crackign, needs attention |
| 105873 | Sagamore Dr. | Baldwin Rd. | Dead End | 0 | 0.47 | 0.47 | 20 | 2 | Asphalt | Vegetation | 2 | 2 | 7/18/2016 | 79 | Patching | 56 | | fields border half, good drainage | No Sidewalk |
| 105898-A | Water St. | Alexandria Ave. | Tin Pan Alley | 0 | 0.15 | 0.15 | 40 | 2 | Asphalt | Paved - Aspahl | 3 | 2 | 7/14/2016 | 83 | Overlay | 54 | | | Tin Pan Side-- 4ft*full length, good condition \\Opposite Side--4ft*full length, good condition |
| 105846-B | Lord Howe St. | Pearl St. | Schuyler St. | 0.5 | 0.64 | 0.14 | 20 | 2 | Asphalt | Curb - Concrete | 4 | 4 | 6/30/2016 | 83 | Surface Treatments | 54 | | | Pearl Side-- 5ft*450ft, great condition, 4ft*130ft, minor cracking overall, 1 bad crack and heave |
| 105842-C | Lake Geroge Ave. | Father Jogues St. | Iroquois St. | 0.09 | 0.14 | 0.05 | 40 | 2 | Asphalt | Curb - Concrete | 4 | 4 | 7/14/2016 | 81 | Surface Treatments | 54 | | | Iroquois Side--5ft*full length, moderate 1"heaves, corner breaks are common \\Opposite Side--5ft, full length, 1"heaves, moderate potholes |
| 105805-D | Calkins Pl. | Newton St. | Lake George Ave. | 0.16 | 0.2 | 0.04 | 42 | 2 | Asphalt | Curb - Concrete | 3 | 5 | 7/14/2016 | 81 | Surface Treatments | 54 | | | Newton Side--4ft, full length, 1 large corner break, good overall \\school Side-- 4ft*335ft, 5ft*145ft, good condition |
| 105811-F | Champlain Ave. | Calkins Pl. | Carillon Rd. | 0.59 | 0.68 | 0.09 | 24 | 2 | Asphalt | Curb - Concrete | 4 | 4 | 7/7/2016 | 77 | Surface Treatments | 54 | | | Side--4ft, full length, minimum edge breaking, good overall |

| | | | | | | | Ticonderoga CAMP-RS Section Report -- (Generated on 07/28/2016) | | | | | | | | | | | | |
|----------|------------------|--------------------|-------------------|-------------|-----------|--------|-----------------------------------------------------------------|-------|--------------|-----------------|-------------------|----------------------|-------------|-----------------|--------------------|----------|-------------------|-------------------------------------------------|-------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|
| | | | | | | | | | | | | | | | | | | | |
| RIN | Name | From | To | Start Miles | End Miles | Length | Width (feet) | Lanes | Surface Type | Shoulder Type | Traffic 1(Lo) - 5 | Importance 1(Lo) - 5 | Survey Date | Condition (PCI) | Repair Category | Priority | Attention Needed? | Road Notes | Sidewalk Notes |
| 105860-B | Old Chilson Rd. | Killicut Mtn. Rd. | County Rt. 56 | 2.78 | 2.9 | 0.12 | 20 | 2 | Asphalt | Vegatation | 1 | 1 | 7/19/2016 | 92 | Crack Repairs | 52 | | | No Sidewalk |
| 105891 | Tiroga Beach Ln. | County Line | County Line | 0 | 0.33 | 0.33 | 16 | 1 | Asphalt | Vegatation | 1 | 1 | 7/18/2016 | 92 | Crack Repairs | 52 | | | No Sidewalk |
| 105880 | Steamboat Lndg. | Baldwin Rd. | Dead End | 0 | 0.13 | 0.13 | 20 | 2 | Asphalt | Vegatation | 1 | 1 | 7/18/2016 | 91 | Patching | 52 | | needs drainage work to help preserve for longer | No Sidewalk |
| 105839 | Keast Rd. | Rt. 74 | Dead End | 0 | 0.1 | 0.1 | 10 | 1 | Unpaved | Vegatation | 1 | 1 | 7/19/2016 | 90 | Surface Reshape | 52 | | | No Sidewalk |
| 105848 | Maplewood Ln. | Shore Airport Rd. | Dead End | 0 | 0.2 | 0.2 | 20 | 2 | Asphalt | Vegatation | 1 | 1 | 7/18/2016 | 89 | Patching | 52 | | | No Sidewalk |
| 105828 | Hall Rd. | Rt. 74 | Dead End | 0 | 0.6 | 0.6 | 20 | 2 | Asphalt | Vegatation | 1 | 1 | 7/19/2016 | 88 | Crack Repairs | 52 | | | No Sidewalk |
| 105822 | Eichen St. | Alexandria Ave. | Dead End | 0 | 0.07 | 0.07 | 16 | 2 | Asphalt | Curb - Concrete | 1 | 1 | 7/19/2016 | 87 | Crack Repairs | 52 | | | No Sidewalk |
| 105826-C | Grace Ave. | St. Clair St. | Morehouse Dr. | 0.1 | 0.13 | 0.03 | 24 | 2 | Asphalt | Curb - Concrete | 1 | 2 | 6/30/2016 | 84 | Patching | 52 | | | Side--4ft*full length, good condition, surface is starting to deteriorate |
| 105860-A | Old Chilson Rd. | Racetrack Rd. | Killicut Mtn. Rd. | 0 | 2.78 | 2.78 | 20 | 2 | Asphalt | Vegatation | 1 | 2 | 7/19/2016 | 82 | Patching | 52 | | | No Sidewalk |
| 105894 | Uncas Dr. | Wiley St. | Grace Ave. | 0 | 0.05 | 0.05 | 20 | 2 | Asphalt | Curb - Concrete | 1 | 2 | 6/30/2016 | 82 | Patching | 52 | | | 5ft*full length, 1 edge break, great condition |
| 105816 | Cottage Rd. | Baldwin Rd. | Hoed Lndg. | 0 | 0.17 | 0.17 | 20 | 2 | Asphalt | Vegatation | 1 | 2 | 7/18/2016 | 77 | Patching | 52 | | | No Sidewalk |
| 105802-I | Burgoyne Rd. | Tower Ave. | Rt.74 & 22 | 0.84 | 1.38 | 0.54 | 22 | 2 | Asphalt | Vegatation | 3 | 4 | 6/30/2016 | 84 | Surface Treatments | 51 | | | starting from Rt. 74, Canal Side--4ft*1500ft, first 500ft, good condition, surface starting to deteriorate, next 230ft, needs work, moderate cracking and ingrown, next 800ft has medium cracking \\ Opposite Side--4ft*500ft, minimum cracking |
| 105792-A | Alexandria Ave. | The Portage | Champlain Ave. | 0 | 0.05 | 0.05 | 43 | 2 | Asphalt | Curb - Concrete | 3 | 4 | 7/14/2016 | 83 | Overlay | 51 | | | Champlain Side--4ft*245ft, 6.5*25ft, very minimal cracking, good condition \\Opposite Side--4ft*125ft, great condition |
| 105887-F | The Portage | Highland St. | Crown Hts. | 0.52 | 0.64 | 0.12 | 32 | 2 | Asphalt | Curb - Concrete | 4 | 3 | 7/5/2016 | 82 | Overlay | 51 | | | Side--4ft*full length, perfect condition |
| 105887-E | The Portage | Carillon Rd. | Highland St. | 0.42 | 0.52 | 0.1 | 30 | 2 | Asphalt | Curb - Concrete | 4 | 3 | 7/5/2016 | 80 | Surface Treatments | 51 | | | Side--4ft*full length, very good condition |
| 105802-H | Burgoyne Rd. | Frasier Bridge Dr. | Tower Ave. | 0.79 | 0.84 | 0.05 | 22 | 2 | Asphalt | Curb - Concrete | 3 | 4 | 6/30/2016 | 78 | Overlay | 51 | | sewer floods over road during storms | Frazier Side--4ft*full length, few cracks, good condition \\Opposite Side-- 4ft*full length, minimum cracking, good condition, needs cleaning |
| 105823-B | Father Jogues | Amherst Ave. | Champlain Ave. | 0.1 | 0.19 | 0.09 | 44 | 2 | Asphalt | Curb - Concrete | 3 | 4 | 7/7/2016 | 75 | Surface Treatments | 51 | YES | | Amherst Side--5ft*full length, good condition, few small heaves \\Opposite Side-- Surface deterioration, rebar exposed |
| 105842-B | Lake George Ave. | Algonkin St. | Father Jogues St. | 0.04 | 0.09 | 0.05 | 40 | 2 | Asphalt | Curb - Concrete | 4 | 4 | 7/14/2016 | 74 | Overlay | 51 | YES | | Algonkin Side--5ft, full length, moderate cracking, consistent, corner breaks \\Opposite Side--5ft*full length, minimum surface deteriorationm, minimal cracking, few 1""heaves |
| 105832 | Hemlock Cir. | Pine Springs | Dead End | 0 | 0.03 | 0.03 | 12 | 1 | Asphalt | Vegatation | 1 | 1 | 7/14/2016 | 85 | Crack Repairs | 48 | | | No Sidewalk |
| 105811-I | Champlain Ave. | Ell St. | Alexandria Ave. | 0.8 | 0.9 | 0.1 | 24 | 2 | Asphalt | Curb - Concrete | 3 | 3 | 7/7/2016 | 84 | Surface Treatments | 48 | | | Side--4ft*full length, minimal surface deterioration, minimal cracking |
| 105811-H | Champlain Ave. | Highland St. | Ell St. | 0.74 | 0.8 | 0.06 | 24 | 2 | Asphalt | Curb - Apshalt | 3 | 3 | 7/7/2016 | 84 | Surface Treatments | 48 | | | Side--4ft*375ft, surface starting to deteriorate, minimal cracking |
| 105801 | Buck Mtn. Rd. | Town Line | Town Line | 0 | 0.44 | 0.44 | 18 | 2 | Unpaved | Vegatation | 1 | 1 | 7/19/2016 | 84 | Surface Reshape | 48 | | lines go from pipe to concrete rock | No Sidewalk |
| 105849 | Mccaughin Rd. | Rt. 74 | Shanahan Rd. | 0 | 0.81 | 0.81 | 20 | 2 | Asphalt | Vegatation | 1 | 1 | 7/18/2016 | 83 | Crack Repairs | 48 | | pasture for shoulder | No Sidewalk |
| 105829-A | Hawkeye | Cannonball | Musketball Run | 0 | 0.05 | 0.05 | 22 | 2 | Asphalt | Vegatation | 2 | 4 | 7/5/2016 | 82 | Surface Treatments | 48 | | | No Sidewalk |
| 105867 | Outlet Dr. | Baldwin Rd. | Dead End | 0 | 0.48 | 0.48 | 20 | 2 | Asphalt | Vegatation | 1 | 1 | 7/18/2016 | 82 | Patching | 48 | | | No Sidewalk |

| | | | | | | | Ticonderoga CAMP-RS Section Report -- (Generated on 07/28/2016) | | | | | | | | | | | | |
|----------|------------------|-------------------|-------------------|-------------|-----------|--------|-----------------------------------------------------------------|-------|--------------|-----------------|-------------------|----------------------|-------------|-----------------|--------------------|----------|-------------------|-----------------------------------------|----------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|
| RIN | Name | From | To | Start Miles | End Miles | Length | Width (feet) | Lanes | Surface Type | Shoulder Type | Traffic 1(Lo) - 5 | Importance 1(Lo) - 5 | Survey Date | Condition (PCI) | Repair Category | Priority | Attention Needed? | Road Notes | Sidewalk Notes |
| 105806 | Canfield Rd. | Putts Pond Rd. | Dead End | 0 | 1.41 | 1.41 | 18 | 2 | Asphalt | Vegetation | 1 | 2 | 7/19/2016 | 82 | Overlay | 48 | | | No Sidewalk |
| 105878-A | Shanahan Rd. | Mccaughlin Rd. | Shore Airport Rd. | 0 | 0.45 | 0.45 | 18 | 2 | Asphalt | Vegetation | 1 | 1 | 7/18/2016 | 81 | Crack Repairs | 48 | | section starts at corner | No Sidewalk |
| 105836 | Howes Lndg. | Baldwin Rd. | Dead End | 0 | 0.24 | 0.24 | 16 | 2 | Asphalt | Vegetation | 1 | 2 | 7/19/2016 | 80 | Overlay | 48 | | | No Sidewalk |
| 105809-D | Cannonball Path | Hawkeye | Montcalm St. | 0.33 | 0.45 | 0.12 | 22 | 2 | Asphalt | Vegetation | 2 | 4 | 7/5/2016 | 78 | Surface Treatments | 48 | | | No Sidewalk |
| 105809-A | Cannonball Path | Champlain Ave. | Hawkeye | 0 | 0.11 | 0.11 | 22 | 2 | Asphalt | Vegetation | 2 | 4 | 7/6/2016 | 75 | Surface Treatments | 48 | | | Side-- 5ft, 230ft, good condition |
| 105823-A | Father Jogues | Champlain Ave. | Amherst Ave. | 0 | 0.1 | 0.1 | 34 | 2 | Asphalt | Curb - Concrete | 3 | 4 | 7/7/2016 | 69 | Surface Treatments | 48 | YES | | Amherst Side--5ft*405ft, 12.5ft*200ft, extreme surface deterioration by driveway, 1" heaving, severe cracking by field \\Opposite Side-- 5ft*185ft, 4.5ft*425ft, minimal heaving <1", severe surface deterioration in one spot, good condition elsewhere |
| 105797-B | Bear Pond Rd. | Pavement Change | Dead End | 0.05 | 1.1 | 1.05 | 16 | 2 | Unpaved | Vegetation | 1 | 1 | 7/19/2016 | 60 | Regrade | 48 | | Services Logging trucks and few camps | No Sidewalk |
| 105881-E | St. Clair St. | Third Street | Grove Ave. | 0.2 | 0.3 | 0.1 | 24 | 2 | Asphalt | Curb - Apshalt | 2 | 3 | 6/29/2016 | 80 | Overlay | 45 | YES | | Side-- 4ft*ends 20ft before grove, few 2" heaves, fair condition |
| 105898-B | Water St. | Tin Pan Alley | The Portage | 0.15 | 0.35 | 0.2 | 34 | 2 | Asphalt | Curb - Concrete | 3 | 2 | 7/14/2016 | 78 | Overlay | 45 | | | Lake Side--4ft, full length, perfect condition \\Opposite Side--5ft*580ft, minimal cracking, few 1" heaves |
| 105868 | Pine Springs | Baldwin Rd. | Dead End? | 0 | 1.3 | 1.3 | 20 | 2 | Asphalt | Vegetation | 1 | 2 | 7/14/2016 | 74 | Overlay | 45 | | | No Sidewalk |
| 105872-B | Rock St. | Overlook Dr. | Woody Ln. | 0.05 | 0.14 | 0.09 | 20 | 2 | Asphalt | Vegetation | 1 | 2 | 7/5/2016 | 71 | Overlay | 45 | | | No Sidewalk |
| 105810 | Center St. | Pine Springs | Pine Springs | 0 | 0.11 | 0.11 | 20 | 2 | Asphalt | Vegetation | 1 | 2 | 7/18/2016 | 88 | Surface Treatment | 42 | | | No Sidewalk |
| 105874 | Sawyer Rd. | Shore Airport Rd. | Rt. 9N / 22 | 0 | 0.25 | 0.25 | 18 | 2 | Asphalt | Vegetation | 1 | 1 | 7/19/2016 | 70 | Overlay | 42 | YES | Extremely Bad Edge Cracking | No Sidewalk |
| 105893 | Treadway St. | The Portage | Woody Ln. | 0 | 0.25 | 0.25 | 28 | 2 | Asphalt | Curb - Concrete | 1 | 2 | 7/5/2016 | 85 | Surface Treatments | 39 | | | Woody Side--4ft*full length, fair condition \\ Opposite Side--4ft*485ft, good condition |
| 105833-B | Highland St. | Champlain Ave. | Ell St. | 0.12 | 0.24 | 0.12 | 26 | 2 | Asphalt | Curb - Concrete | 1 | 2 | 7/7/2016 | 84 | Surface Treatments | 39 | YES | | Side--4ft*50ft, moderate cracking overall, some severe corner breaks |
| 105809-B | Cannonball Path | Hawkeye St. | Musketball Path | 0.11 | 0.22 | 0.11 | 22 | 2 | Asphalt | Vegetation | 1 | 2 | 7/5/2016 | 82 | Surface Treatments | 39 | | | No Sidewalk |
| 105821 | Ell St. | Champlain Ave. | Highland St. | 0 | 0.15 | 0.15 | 22 | 2 | Asphalt | Vegetation | 1 | 2 | 7/7/2016 | 81 | Surface Treatments | 39 | YES? | No drains present, yard ponding | No Sidewalk |
| 105835 | Holcomb Ave. | Iroquois St. | Dead End | 0 | 0.09 | 0.09 | 24 | 2 | Asphalt | Curb - Concrete | 1 | 2 | 7/7/2016 | 78 | Overlay | 39 | | | Side--4ft*full length, <1" heaves, 1 severe edge crack |
| 105800-A | Bull Rock Rd. | Rt. 9N | Shattuck Rd. | 0 | 0.68 | 0.68 | 18 | 2 | Asphalt | Vegetation | 1 | 2 | 7/19/2016 | 75 | Overlay | 39 | | floods on occasion | No Sidewalk |
| 105817 | Crown Hts. | The Portage | Dead End | 0 | 0.15 | 0.15 | 20 | 2 | Asphalt | Curb - Apshalt | 1 | 2 | 7/6/2016 | 75 | Surface Treatments | 39 | | | No Sidewalk |
| 105831 | Heather Hts. | Burgoyne Rd. | Dead End | 0 | 0.08 | 0.08 | 20 | 2 | Asphalt | Vegetation | 1 | 1 | 6/30/2016 | 65 | Overlay | 39 | | | No Sidewalk |
| 105842-E | Lake Geroqe Ave. | Stanton St. | Calkins Pl. | 0.31 | 0.51 | 0.2 | 40 | 2 | Asphalt | Curb - Concrete | 4 | 5 | 7/14/2016 | 77 | Rehab | 38 | YES | | Calkins Side--5ft*full length, 440ft is severe potholes & surface deterioration \\Opposite Side-- 5ft*675ft, moderate cracking in some areas, fair condition |
| 105808 | Carnegie Place | Montcalm St. | Municipal Pk. L. | 0 | 0.03 | 0.03 | 26 | 2 | Asphalt | Curb - Concrete | 2 | 4 | 6/29/2016 | 76 | Rehab | 38 | | | Side--5ft*105ft, minor edge cracking, good condition |
| 105802-A | Burgoyne Rd. | Rt. 74 & 22 | Park Ave. | 0 | 0.04 | 0.04 | 24 | 2 | Asphalt | Vegetation | 4 | 3 | 6/30/2016 | 68 | Rehab | 38 | | | No Sidewalk |
| 105869 | Price Rd. | Rt. 9N | Dead End | 0 | 0.2 | 0.2 | 16 | 2 | Asphalt | Vegetation | 1 | 1 | 7/19/2016 | 77 | Overlay | 36 | | Shifts to unpaved, still good condition | No Sidewalk |
| 105844 | Lee Ln. | Pine Springs | Dead End | 0 | 0.03 | 0.03 | 16 | 1 | Unpaved | Vegetation | 1 | 1 | 8/2/2016 | 75 | Regrade | 36 | | Switches to pavement | No Sidewalk |
| 105826-A | Grace Ave. | Dead End | Uncas Dr. | 0 | 0.06 | 0.06 | 22 | 2 | Asphalt | Curb - Concrete | 1 | 2 | 6/30/2016 | 69 | Overlay | 36 | | | Uncas Side-- 5ft*200ft, fair condition \\Opposite Side-- 5ft*230, good, 4ft*80ft, poor cracking along 10ft |
| 105809-C | Cannonball Path | Musketball Run | Hawkeye | 0.22 | 0.33 | 0.11 | 22 | 2 | Asphalt | Vegetation | 1 | 2 | 7/5/2016 | 68 | Overlay | 36 | | | No Sidewalk |

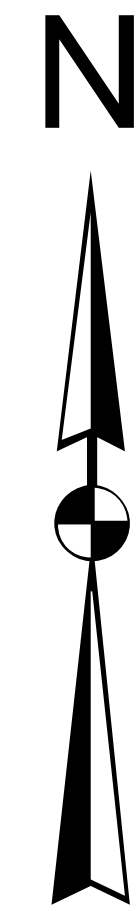
| | | | | | | Ticonderoga CAMP-RS Section Report -- (Generated on 07/28/2016) | | | | | | | | | | | | | |
|----------|-----------------|-----------------|------------------|----------------|--------------|-----------------------------------------------------------------|-----------------|-------|-----------------|------------------|----------------------|-------------------------|----------------|--------------------|-------------------|----------|----------------------|-----------------------------------------------------------|--------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|
| RIN | Name | From | To | Start Miles | End Miles | Length | Width (feet) | Lanes | Surface Type | Shoulder Type | Traffic 1(Lo) - 5 | Importance 1(Lo) - 5 | Survey Date | Condition (PCI) | Repair Category | Priority | Attention Needed? | Road Notes | Sidewalk Notes |
| 105802-F | Burgoyne Rd. | Lonergan Ln. | Heather Hts. | 0.62 | 0.72 | 0.1 | 22 | 2 | Asphalt | Vegetation | 3 | 4 | 6/30/2016 | 65 | Rehab | 36 | | | Side--4ft*110ft, minimum cracking, small heaves, fair condition, 5ft*340ft, perect condition |
| 105818-A | Defiance St. | The Portage | Cossey St. | 0 | 0.19 | 0.19 | 26 | 2 | Asphalt | Vegetation | 2 | 3 | 7/5/2016 | 73 | Rehab | 34 | | | Side--4ft*160ft, 14ft of extreme surface deterioration, rest is great condition |
| 105818-C | Defiance St. | Mt.Defiance Rd. | Dead End | 0.34 | 0.44 | 0.1 | 14 | 2 | Asphalt | Vegetation | 1 | 1 | 7/5/2016 | 70 | Rehab | 34 | | | No Sidewalk |
| 105861 | Overlook Dr. | Rock St. | Montcalm St. | 0 | 0.3 | 0.3 | 20 | 2 | Asphalt | Vegetation | 1 | 2 | 7/5/2016 | 61 | Rehab | 34 | | | No Sidewalk |
| 105872-A | Rock St. | Coeesy St | Overlook Dr. | 0 | 0.05 | 0.05 | 20 | 2 | Asphalt | Curb - Concrete | 1 | 2 | 7/5/2016 | 75 | Rehab | 32 | | | No Sidewalk |
| 105858 | Musketballl Run | Canonball Run | Hawkeye Ln. | 0 | 0.08 | 0.08 | 22 | 2 | Asphalt | Vegetation | 1 | 1 | 7/5/2016 | 64 | Rehab | 32 | YES? | road is closed | No Sidewalk |
| 105799 | Bernetz Rd. | Rt. 9N | Dead End | 0 | 0.1 | 0.1 | 16 | 2 | Asphalt | Vegetation | 1 | 1 | 7/19/2016 | 78 | Rehab | 30 | | Road Extending to bridge is not a drivable road | No Sidewalk |
| 105826-B | Grace Ave. | Uncas Dr. | St.Clair St. | 0.06 | 0.1 | 0.04 | 26 | 2 | Asphalt | Vegetation | 1 | 2 | 6/30/2016 | 71 | Rehab | 30 | | | Side--5ft*full length, great condition |
| 105818-B | Defiance St. | Cossey St. | Mt.Defiance Rd. | 0.19 | 0.34 | 0.15 | 24 | 2 | Asphalt | Vegetation | 2 | 2 | 7/5/2016 | 63 | Rehab | 30 | | | Side--4ft*255ft, minimum heaving, few edge breaks |
| 105793 | Algonkin St. | Champlain Ave. | Lake George Ave. | 0 | 0.13 | 0.13 | 28 | 1 | Asphalt | Curb - Concrete | 2 | 3 | 7/6/2016 | 54 | Rehab | 30 | YES | | Laundromat Side--4ft*445ft, fair condition overall, 20ft needs attention, 10ft surface gone \\Opposite Side--4ft*full length, 2" heaving & surface deterioration by driveways, edge breaks |
| 105883 | Stony Lonesome | Corduroy Rd. | Town Line | 0 | 0.52 | 0.52 | 20 | 2 | Asphalt | Vegetation | 1 | 1 | 7/19/2016 | 52 | Rehab | 30 | | | No Sidewalk |
| 105850 | McCormick St. | Champlain Ave. | Tower Ave. | 0 | 0.06 | 0.06 | 18 | 2 | Asphalt | Curb - Concrete | 2 | 2 | 6/30/2016 | 77 | Rehab | 28 | | | Parking Side--4ft*295ft, minimum cracking, edge covered with earth ~3"', 5ft*35ft, perfect condition \\Opposite Side--6ft*135ft, good condition |
| 105797-A | Bear Pond Rd. | Rt. 74 | Pavement Change | 0 | 0.05 | 0.05 | 16 | 2 | Asphalt | Vegetation | 1 | 1 | 7/19/2016 | 73 | Rehab | 28 | | Services logging trucks and some camps | No Sidewalk |
| 105815-B | Cossey St. | Meyer St. | Rock St. | 0.05 | 0.3 | 0.25 | 16 | 2 | Asphalt | Curb - Concrete | 1 | 2 | 7/5/2016 | 65 | Rehab | 28 | | | Side--4ft*960ft, minor heaving, minor cracking |
| 105815-C | Cossey St. | Rock St. | Defiance St. | 0.3 | 0.55 | 0.25 | 20 | 2 | Asphalt | Vegetation | 1 | 2 | 7/5/2016 | 63 | Rehab | 28 | YES | | Side--3.5ft*350ft, moderate cracking, some surface deterioration |
| 105825 | George St. | Lord Howe St. | John St. | 0 | 0.15 | 0.15 | 30 | 2 | Asphalt | Curb - Concrete | 1 | 2 | 6/30/2016 | 60 | Rehab | 28 | YES | | George Side--4ft*full length, poor joint sealing, bad heaving, sections coming apart with 2"-4" gaps |
| 105800-B | Bull Rock Rd. | Shattuck Rd. | Dead End | 0.68 | 1.2 | 0.52 | 18 | 2 | Asphalt | Vegetation | 1 | 2 | 7/19/2016 | 58 | Rehab | 28 | YES | Culverts are not paved over | No Sidewalk |
| 105810-B | Birchwood Rd. | Kennedy Dr. | Dead End | 0 | 0.02 | 0.02 | 16 | 2 | Asphalt | Vegetation | 1 | 2 | 7/19/2016 | 57 | Rehab | 28 | YES | Section of Birchwood is a local road, very poor condition | No Sidewalk |
| 105815-A | Cossey St. | Montcalm St. | Meyer St. | 0 | 0.05 | 0.05 | 20 | 2 | Asphalt | Vegetation | 2 | 2 | 7/5/2016 | 69 | Rehab | 26 | | | Side--4ft*full length, good condition, minimum cracking |
| 105820 | Depot St. | Champlain Ave. | Comm. Bldg. Lot | 0 | 0.08 | 0.08 | 16 | 2 | Asphalt | Earth | 1 | 1 | 7/6/2016 | 65 | Rehab | 26 | | | Asphalt Side--6ft*110ft |
| 105852 | Mohawk Trail | Mt. Hope Ave. | Grace Ave. | 0 | 0.07 | 0.07 | 18 | 2 | Asphalt | Vegetation | 1 | 1 | 6/30/2016 | 60 | Rehab | 26 | | | No Sidewalk |
| 105811-B | Champlain Ave. | Montcalm St. | Algonkin St. | 0.04 | 0.13 | 0.09 | 50 | 2 | Asphalt | Curb - Concrete | 5 | 5 | 7/7/2016 | 94 | Defer Maintenance | 21 | | | Sunoco Side-- 8ft*140ft, 9ft*90ft, 5.5ft*65ft, 7ft*50ft, \\Post Office Side-- 5.5ft*160ft, 10ft*245ft, minimum surface deterioration |
| 105892-B | Tower Ave. | McCormick St. | Burgoyne Rd. | 0.05 | 0.15 | 0.1 | 26 | 2 | Asphalt | Curb - Granite | 4 | 5 | 6/30/2016 | 94 | Defer Maintenance | 20 | | Road and Sidewalk new 2015 | Side--5ft*full length, perfect condition |
| 105892-A | Tower Ave. | Montcalm St. | McCormick St. | 0 | 0.05 | 0.05 | 26 | 2 | Asphalt | Curb - Granite | 4 | 5 | 6/30/2016 | 94 | Defer Maintenance | 20 | | Road and Sidewalk new 2015 | Side--5ft*full length, perfect condition |

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|----------|-----------------|-------------------|-----------------|-------------|-----------|-----------------------------------------------------------------|--------------|-------|--------------|-----------------|-------------------|----------------------|-------------|-----------------|-------------------|----------|-------------------|-----------------------------------------------------------------------|--------------------------------------------------------------------------------------------------------------------------------------------------------------|
| RIN | Name | From | To | Start Miles | End Miles | Length | Width (feet) | Lanes | Surface Type | Shoulder Type | Traffic 1(Lo) - 5 | Importance 1(Lo) - 5 | Survey Date | Condition (PCI) | Repair Category | Priority | Attention Needed? | Road Notes | Sidewalk Notes |
| 105888-A | Third Ave. | St. Clair St. | Summit St. | 0 | 0.1 | 0.1 | 28 | 2 | Asphalt | Curb - Apshalt | 2 | 3 | 6/29/2016 | 94 | Defer Maintenance | 16 | YES | Recenetly Paved | Side-- 4ft*full length, bad surface, poor cracking, poor heaving near driveways, many moderate cracks |
| 105881-C | St. Clair St. | Wayne Ave | Second St | 0.1 | 0.15 | 0.05 | 28 | 2 | Asphalt | Curb - Concrete | 2 | 3 | 6/29/2016 | 94 | Defer Maintenance | 16 | | | Second Side-- 5ft*full length, minimum cracking, fair condition \\Opposite Side-- 4ft*133ft, few cracks, good condition |
| 105899-D | Wayne Ave. | Summit St. | Firemans Field | 0.37 | 0.43 | 0.06 | 20 | 2 | Asphalt | Vegatation | 2 | 2 | 6/29/2016 | 94 | Defer Maintenance | 15 | | | No Sidewalk |
| 105886-B | Sunset St. | Grace Ave. | Park Ave. | 0.08 | 0.12 | 0.04 | 18 | 2 | Asphalt | Vegatation | 2 | 2 | 6/30/2016 | 94 | Defer Maintenance | 15 | | Recenetly Paved | No Sidewalk |
| 105886-A | Sunset St. | Mt. Hope Ave. | Grave Ave. | 0 | 0.08 | 0.08 | 18 | 2 | Asphalt | Vegatation | 2 | 2 | 6/30/2016 | 94 | Defer Maintenance | 15 | | Recenetly Paved | No Sidewalk |
| 105885-B | Summit St. | Wayne Ave. | Park Ave. | 0.1 | 0.15 | 0.05 | 20 | 2 | Asphalt | Pave - Concrete | 2 | 2 | 6/28/2016 | 94 | Defer Maintenance | 15 | YES | Road in great shape | Side-- 4ft*155ft, poor surface condition, severely cracked in some areas, needs attention in front of 2 houses |
| 105885-A | Summit St. | Third St. | Wayne Ave. | 0 | 0.1 | 0.1 | 20 | 2 | Asphalt | Vegatation | 2 | 2 | 6/29/2016 | 94 | Defer Maintenance | 15 | | Road in great shape | No Sidewalk |
| 105862-B | Park Ave. | Summit St. | Sunset St. | 0.1 | 0.34 | 0.24 | 24 | 2 | Asphalt | Vegatation | 2 | 2 | 6/29/2016 | 94 | Defer Maintenance | 15 | | Recenetly Paved | Side-- 4ft*400ft, minimal cracking, good condition |
| 105862-A | Park Ave. | St. Clair St. | Summit St. | 0 | 0.1 | 0.1 | 28 | 2 | Asphalt | Curb - Concrete | 2 | 2 | 6/29/2016 | 94 | Defer Maintenance | 15 | YES | Recenetly Paved | Summit Side-- 4ft*370ft, minimal cracking, good condition \\ Opposite Side-- 4ft*full length, 1 edge break needs attention, minimal cracking, good condition |
| 105856-B | Mt. Hope Ave. | Morehouse Dr. | Patriot Pass | 0.1 | 0.2 | 0.1 | 28 | 2 | Asphalt | Vegatation | 2 | 2 | 6/30/2016 | 94 | Defer Maintenance | 15 | | Recenetly Paved | No Sidewalk |
| 105878-C | Shanahan Rd. | Avaition Rd. | Delano Rd. | 0.69 | 1.35 | 0.66 | 18 | 2 | Asphalt | Vegatation | 1 | 2 | 7/18/2016 | 94 | Defer Maintenance | 14 | | | No Sidewalk |
| 105838-B | John St. | George St. | Schuyler St. | 0.1 | 0.25 | 0.15 | 28 | 2 | Asphalt | Curb - Concrete | 1 | 2 | 6/30/2016 | 94 | Defer Maintenance | 14 | | | John Side--5ft*whole length both sides, perfect condition |
| 105840 | Kennedy Dr. | Alexandria Ave. | Birchwood Rd. | 0 | 0.09 | 0.09 | 20 | 2 | Asphalt | Vegatation | 1 | 2 | 7/19/2016 | 94 | Defer Maintenance | 14 | YES | Section at Kennedy Birchwood intersection, poor condition | No Sidewalk |
| 105791 | Abercrombie St. | The Portage | Dead End | 0 | 0.18 | 0.18 | 28 | 2 | Asphalt | Vegatation | 1 | 2 | 7/6/2016 | 94 | Defer Maintenance | 14 | | | No Sidewalk |
| 105889 | Timber St. | Woody Ln. | Dead End | 0 | 0.02 | 0.02 | 14 | 1 | Asphalt | Vegatation | 1 | 1 | 7/5/2016 | 36 | Reconstruction | 14 | YES | Road almost umpassable, terrible condition but only services 2 houses | No Sidewalk |
| 105879 | Spruce Cir. | Pine Springs | Dead End | 0 | 0.03 | 0.03 | 12 | 1 | Asphalt | Vegatation | 1 | 1 | 8/2/2016 | 94 | Defer Maintenance | 13 | | Listed as a local road but has a private road sign up | No Sidewalk |
| 105897-A | Warner Hill Rd. | Rt. 9N / 22 | Pavement Change | 0 | 2.02 | 2.02 | 20 | 2 | Asphalt | Vegatation | 1 | 1 | 7/19/2016 | 94 | Defer Maintenance | 13 | | | No Sidewalk |
| 105876 | Second Ave. | St. Clair St. | Dead End | 0 | 0.05 | 0.05 | 16 | 2 | Asphalt | Paved - Aspahl | 1 | 1 | 6/29/2016 | 94 | Defer Maintenance | 13 | | Dead End covered in sediment | No Sidewalk |
| 105855 | Mossy Point Rd. | Black Point | Black Point | 0 | 0.47 | 0.47 | 20 | 2 | Asphalt | Vegatation | 1 | 1 | 7/14/2016 | 94 | Defer Maintenance | 13 | | Recently paved and ditched | No Sidewalk |
| 105803-A | Birchwood Rd. | Kennedy Dr. | Dead End | 0 | 0.15 | 0.15 | 16 | 2 | Asphalt | Vegatation | 1 | 1 | 7/19/2016 | 94 | Defer Maintenance | 13 | | | No Sidewalk |
| 105819 | Sawyer Farm Rd. | Shore Airport Rd. | Dead End | 0 | 0.65 | 0.65 | 20 | 2 | Asphalt | Vegatation | 1 | 2 | 7/19/2016 | 23 | Reconstruction | 13 | | Not listed in local roads listing, covered in sediment | No Sidewalk |
| 105845 | Lester Hack Rd. | Shore Airport Rd. | Dead End | 0 | 0.2 | 0.2 | 20 | 2 | Asphalt | Vegatation | 1 | 1 | 7/19/2016 | 51 | Reconstruction | 12 | | Not the best crown | No Sidewalk |
| 257731-B | Birnbaum Rd. | Closed Road | Closed Road | 0.76 | 0.86 | 0.1 | 32 | 2 | Unpaved | Vegatation | 1 | 1 | 7/19/2016 | 18 | Reconstruction | 12 | | This is a closed road | No Sidewalk |
| 257731-A | Birnbaum Rd. | Rt. 9N / 22 | Closed Road | 0 | 0.76 | 0.76 | 32 | 2 | Unpaved | Vegatation | 1 | 1 | 7/19/2016 | 15 | Reconstruction | 12 | | Services no houses | No Sidewalk |



Appendix B

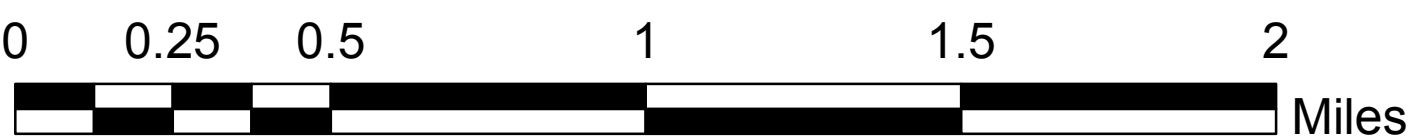
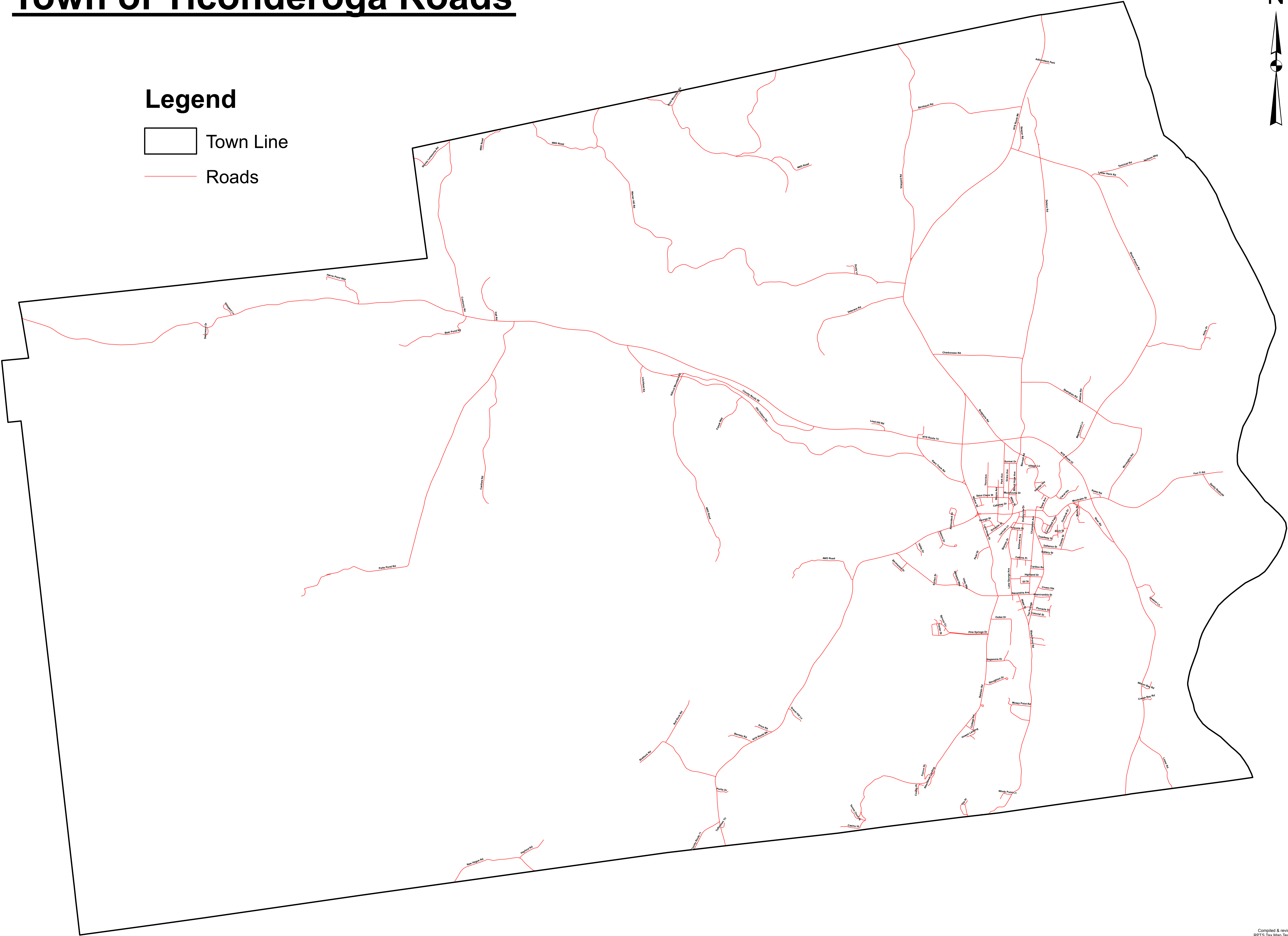
Ticonderoga Map of Local Roads

Town of Ticonderoga Roads

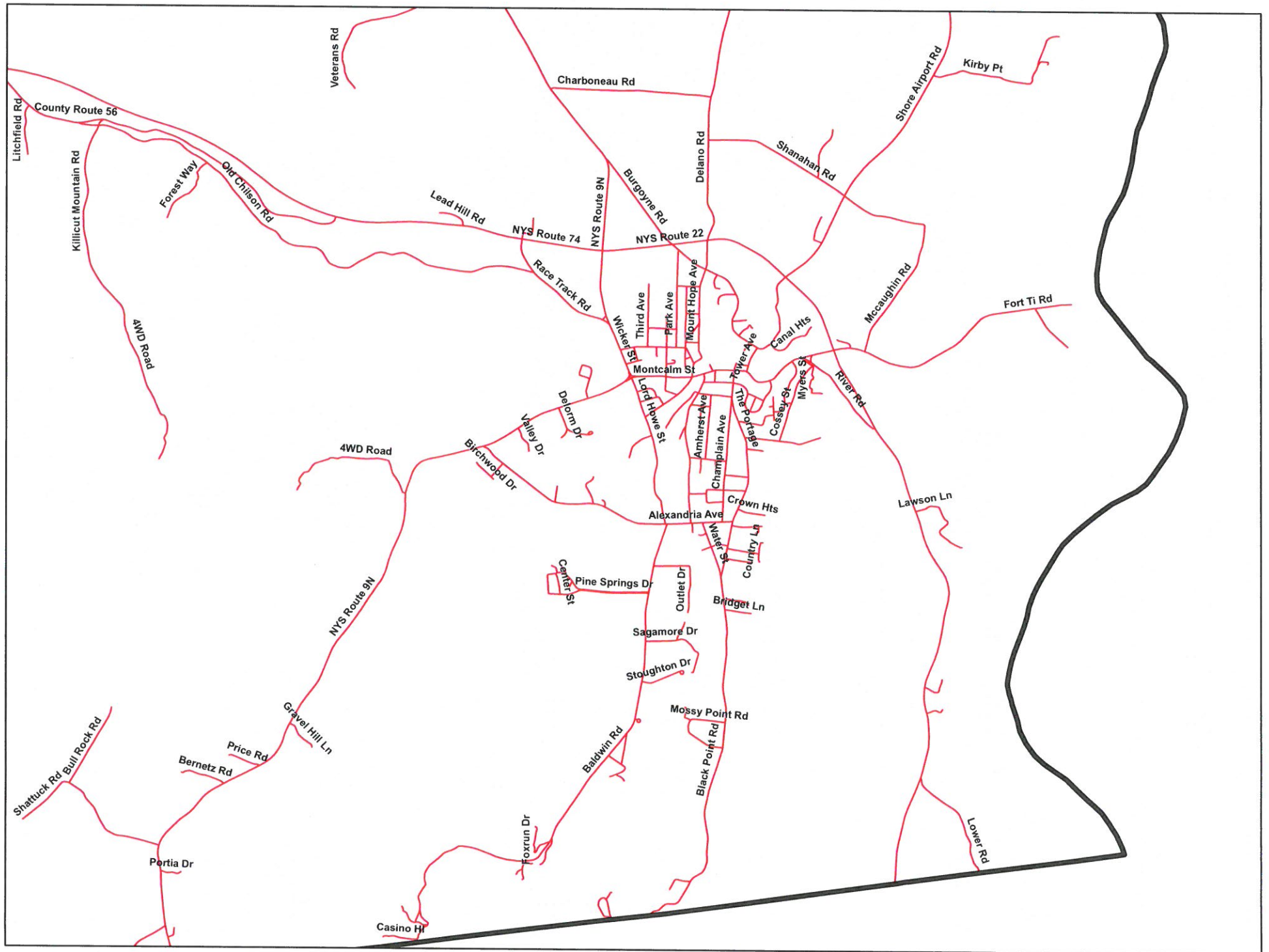


Legend

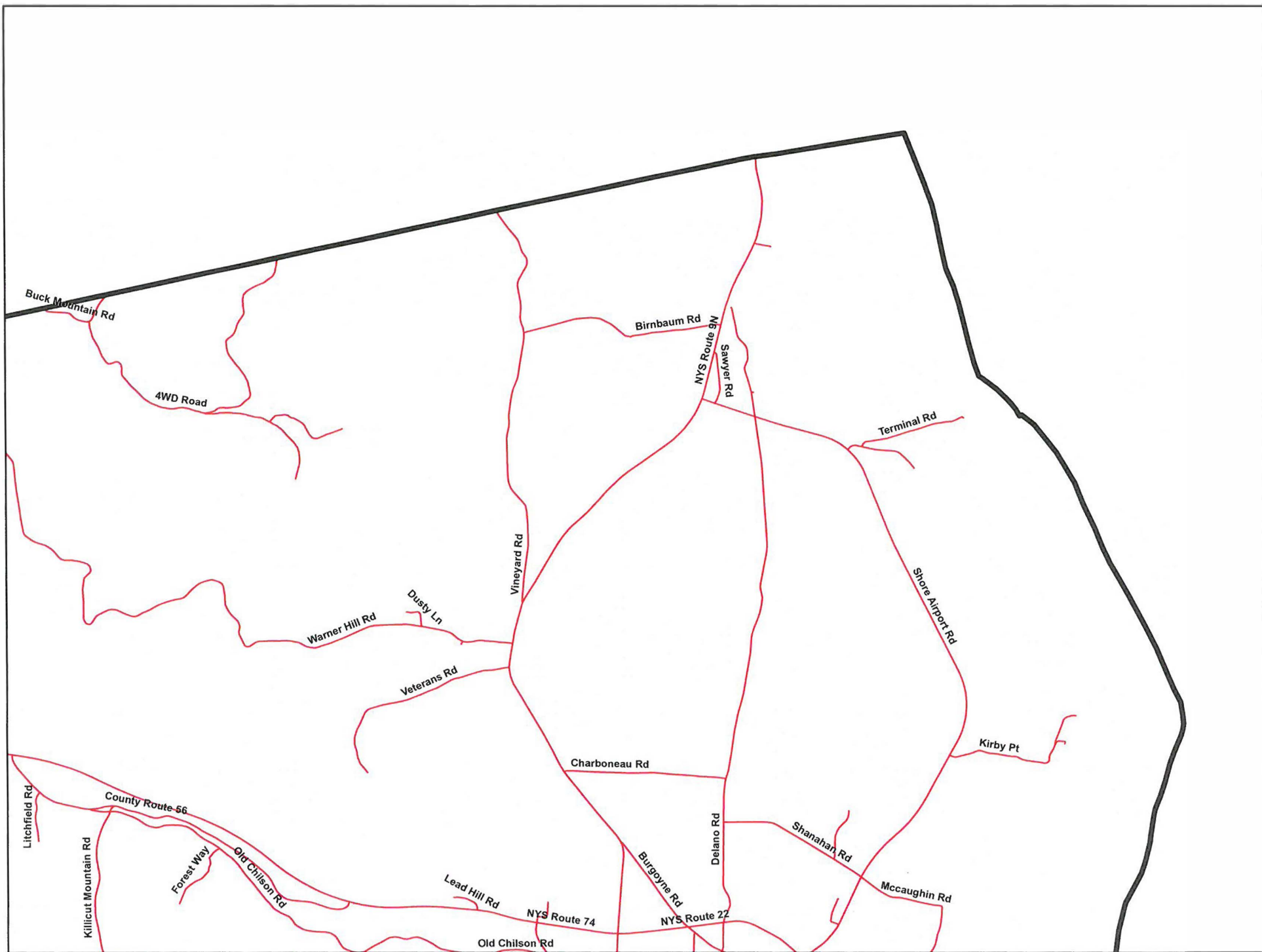
-  Town Line
-  Roads



Compiled & revised by T.J. Gay
RP/TS Tax Map Tech 1, on 01/17/2016
Information from various sources and dates.
Essex County cannot and does not guarantee nor
otherwise acknowledge the accuracy or reliability
of the data or responsibility for the use of the
data and for any reliance placed upon the data or
upon derivations of the data. Any distribution or
copying of the data requires the expressed written
permission of the
Essex County Real Property Tax Services Agency

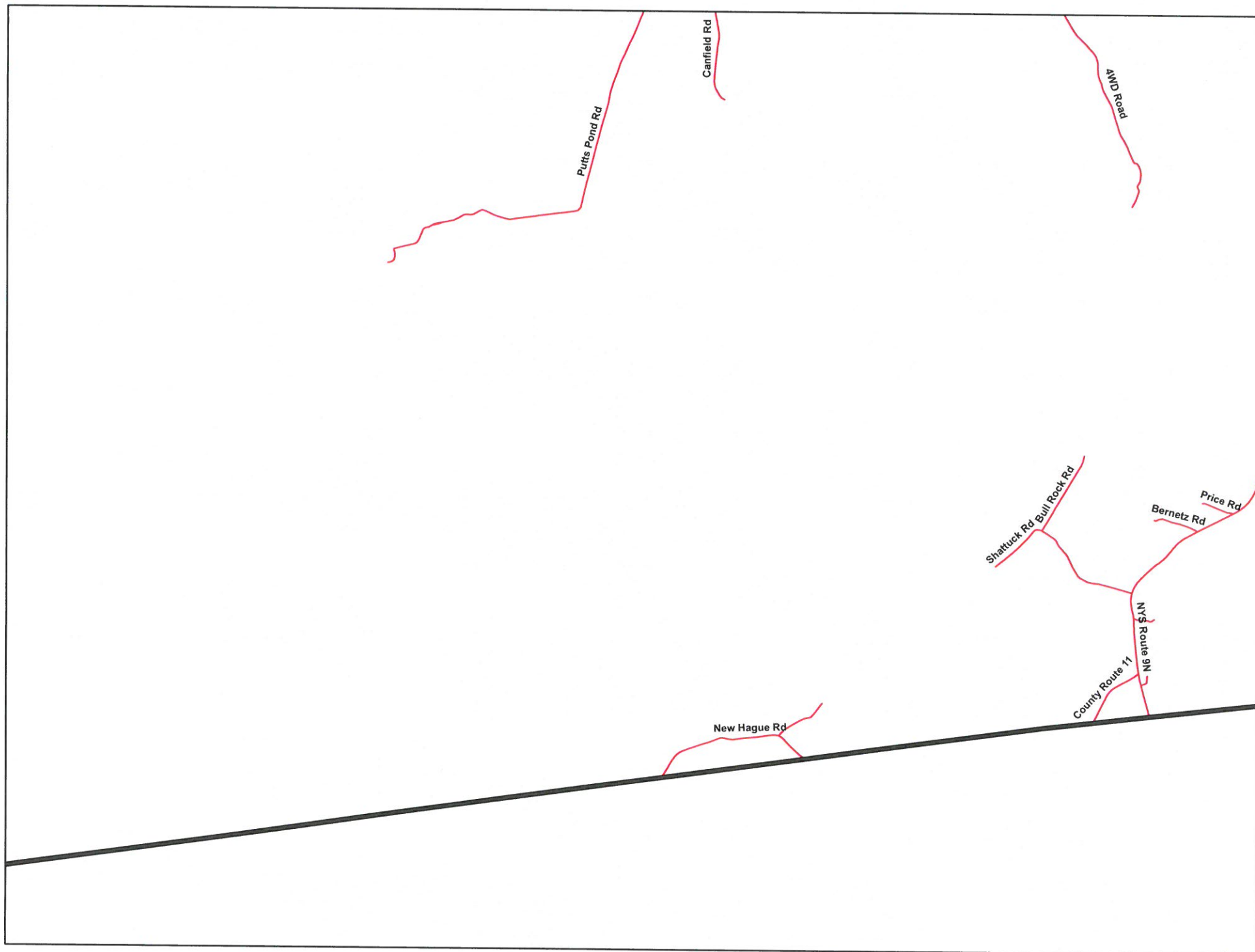


Town : Southeast Corner

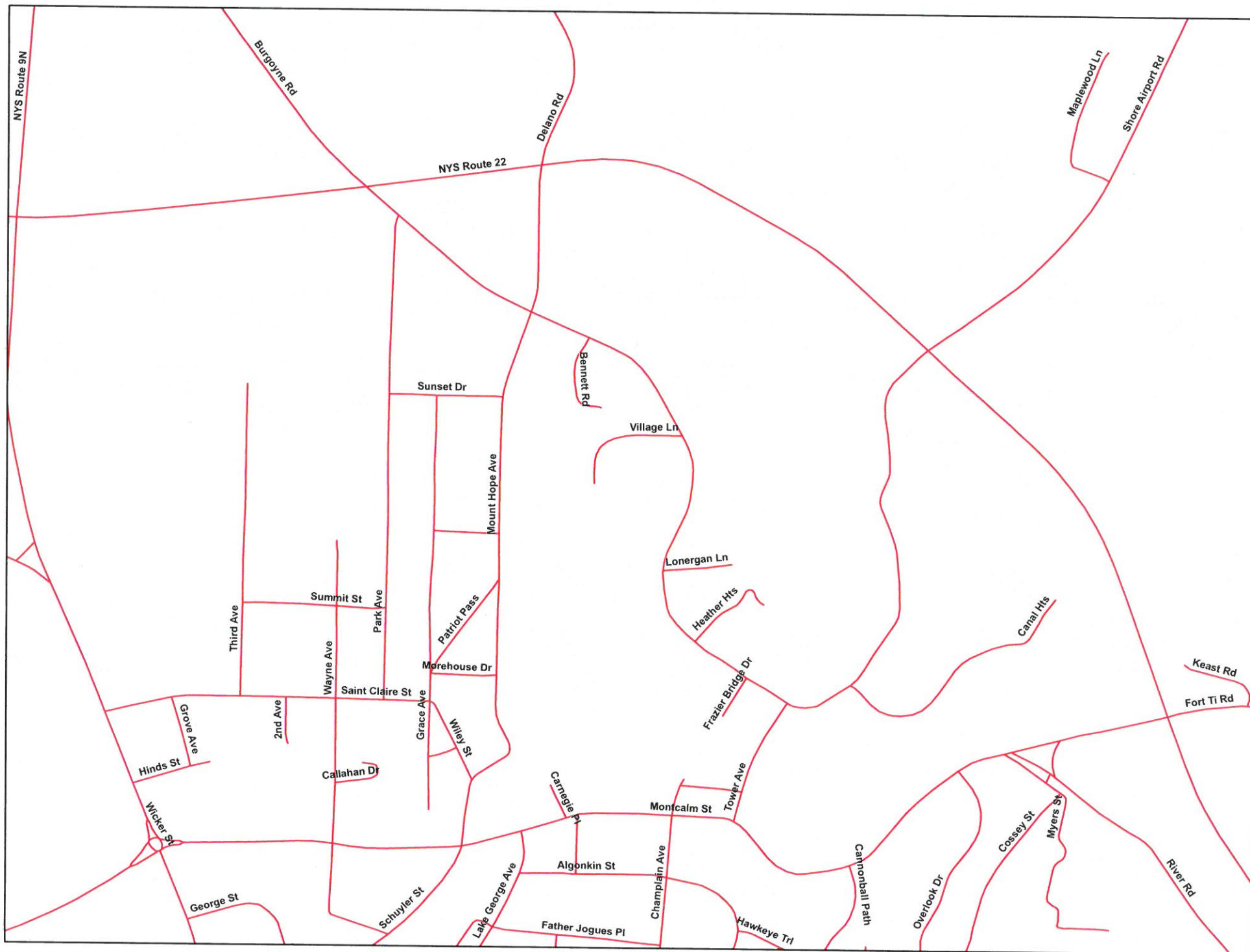


Town : Northeast Corner

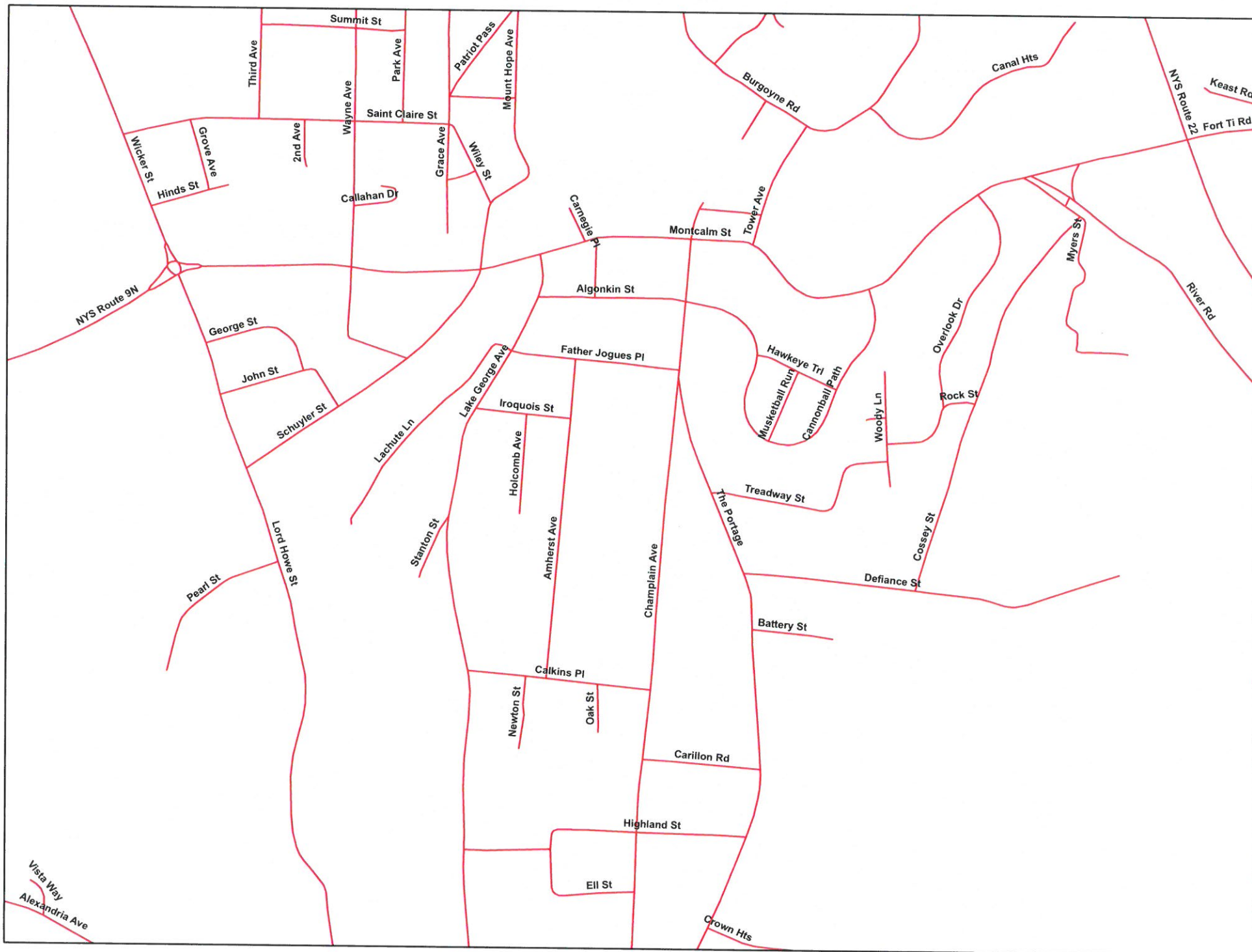




Town : Southwest Corner



Hamlet : Northern Section









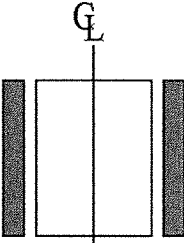
Hamlet : Southern Section

Appendix C

CAMP-RS Asphalt Pavement Condition Survey

CAMP-RS Asphalt Pavement Condition Survey

| | | |
|------------------|-----------------|---------------------|
| Street: _____ | Distance: _____ | Name: _____ |
| Section #: _____ | Start: _____ | Date: _____ |
| Start: _____ | End: _____ | Weather: _____ |
| End: _____ | Length: _____ | Temp (F°/C°): _____ |


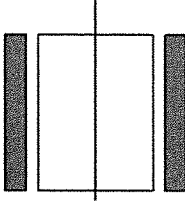

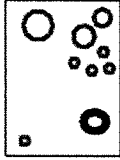



| | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
|--------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|------------|---|---|--------|---|------|--------------|------|------|------|------|------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|-----------|---|---|------|---|------|-----|------|------|-------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|------------|--|--------|--------|---|------|--------------|--------|------|------|------|---|---|---|---|---|---|---|-----|-----|------|
| <p>LONGITUDINAL/ TRANSVERSE CRACKING</p> <div style="display: flex; align-items: center;">  <div style="margin-left: 10px;"> <table border="1" style="border-collapse: collapse; text-align: center;"> <tr><td colspan="3">NO Defects</td></tr> <tr><td colspan="3">EXTENT</td></tr> <tr><td colspan="3">Low Med High</td></tr> <tr><td>1</td><td>2</td><td>3</td></tr> <tr><td>4</td><td>5</td><td>6</td></tr> <tr><td>7</td><td>8</td><td>9</td></tr> </table> </div> </div> <div style="display: flex; align-items: center; margin-top: 10px;"> <div style="writing-mode: vertical-rl; transform: rotate(180deg); font-weight: bold; margin-right: 5px;">SEVERITY</div> <table style="margin-right: 10px;"> <tr><td>Low</td></tr> <tr><td>Med</td></tr> <tr><td>High</td></tr> </table> </div> | NO Defects | | | EXTENT | | | Low Med High | | | 1 | 2 | 3 | 4 | 5 | 6 | 7 | 8 | 9 | Low | Med | High | <p>ALLIGATOR CRACKING</p> <div style="display: flex; align-items: center;">  <div style="margin-left: 10px;"> <table border="1" style="border-collapse: collapse; text-align: center;"> <tr><td colspan="3">NO Defects</td></tr> <tr><td colspan="3">EXTENT</td></tr> <tr><td colspan="3">Low Med High</td></tr> <tr><td>1</td><td>2</td><td>3</td></tr> <tr><td>4</td><td>5</td><td>6</td></tr> <tr><td>7</td><td>8</td><td>9</td></tr> </table> </div> </div> <div style="display: flex; align-items: center; margin-top: 10px;"> <div style="writing-mode: vertical-rl; transform: rotate(180deg); font-weight: bold; margin-right: 5px;">SEVERITY</div> <table style="margin-right: 10px;"> <tr><td>Low</td></tr> <tr><td>Med</td></tr> <tr><td>High</td></tr> </table> </div> | NO Defects | | | EXTENT | | | Low Med High | | | 1 | 2 | 3 | 4 | 5 | 6 | 7 | 8 | 9 | Low | Med | High |
| NO Defects | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| EXTENT | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| Low Med High | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 1 | 2 | 3 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 4 | 5 | 6 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 7 | 8 | 9 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| Low | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| Med | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| High | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| NO Defects | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| EXTENT | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| Low Med High | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 1 | 2 | 3 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 4 | 5 | 6 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 7 | 8 | 9 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| Low | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| Med | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| High | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| <p>EDGE CRACKING</p> <div style="display: flex; align-items: center;">  <div style="margin-left: 10px;"> <table border="1" style="border-collapse: collapse; text-align: center;"> <tr><td colspan="3">NO Defects</td></tr> <tr><td colspan="3">EXTENT</td></tr> <tr><td colspan="3">Low Med High</td></tr> <tr><td>1</td><td>2</td><td>3</td></tr> <tr><td>4</td><td>5</td><td>6</td></tr> <tr><td>7</td><td>8</td><td>9</td></tr> </table> </div> </div> <div style="display: flex; align-items: center; margin-top: 10px;"> <div style="writing-mode: vertical-rl; transform: rotate(180deg); font-weight: bold; margin-right: 5px;">SEVERITY</div> <table style="margin-right: 10px;"> <tr><td>Low</td></tr> <tr><td>Med</td></tr> <tr><td>High</td></tr> </table> </div> | NO Defects | | | EXTENT | | | Low Med High | | | 1 | 2 | 3 | 4 | 5 | 6 | 7 | 8 | 9 | Low | Med | High | <p>PATCHING / POTHoles</p> <div style="display: flex; align-items: center;">  <div style="margin-left: 10px;"> <table border="1" style="border-collapse: collapse; text-align: center;"> <tr><td colspan="2">NO Defects</td></tr> <tr><td colspan="2">EXTENT</td></tr> <tr><td>1</td><td>Low</td></tr> <tr><td>2</td><td>Medium</td></tr> <tr><td>3</td><td>High</td></tr> </table> </div> </div> <p style="font-size: small; margin-top: 5px;">Do not include good patches</p> | NO Defects | | EXTENT | | 1 | Low | 2 | Medium | 3 | High | | | | | | | | | | | |
| NO Defects | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| EXTENT | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| Low Med High | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 1 | 2 | 3 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 4 | 5 | 6 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 7 | 8 | 9 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| Low | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| Med | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| High | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| NO Defects | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| EXTENT | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 1 | Low | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 2 | Medium | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 3 | High | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| <p>RUTTING</p> <div style="display: flex; align-items: center;">  <div style="margin-left: 10px;"> <table border="1" style="border-collapse: collapse; text-align: center;"> <tr><td colspan="3">NO Defects</td></tr> <tr><td colspan="3">EXTENT</td></tr> <tr><td colspan="3">Low Med High</td></tr> <tr><td>1</td><td>2</td><td>3</td></tr> <tr><td>4</td><td>5</td><td>6</td></tr> <tr><td>7</td><td>8</td><td>9</td></tr> </table> </div> </div> <div style="display: flex; align-items: center; margin-top: 10px;"> <div style="writing-mode: vertical-rl; transform: rotate(180deg); font-weight: bold; margin-right: 5px;">SEVERITY</div> <table style="margin-right: 10px;"> <tr><td>Low</td></tr> <tr><td>Med</td></tr> <tr><td>High</td></tr> </table> </div> | NO Defects | | | EXTENT | | | Low Med High | | | 1 | 2 | 3 | 4 | 5 | 6 | 7 | 8 | 9 | Low | Med | High | <p>BLEEDING</p> <div style="display: flex; align-items: center;">  <div style="margin-left: 10px;"> <table border="1" style="border-collapse: collapse; text-align: center;"> <tr><td colspan="2">CONDITION</td></tr> <tr><td>1</td><td>Good</td></tr> <tr><td>4</td><td>Fair</td></tr> <tr><td>7</td><td>Poor</td></tr> </table> </div> </div> <div style="display: flex; align-items: center; margin-top: 10px;"> <div style="writing-mode: vertical-rl; transform: rotate(180deg); font-weight: bold; margin-right: 5px;">SEVERITY</div> <table style="margin-right: 10px;"> <tr><td>Good</td></tr> <tr><td>Fair</td></tr> <tr><td>Poor</td></tr> </table> </div> | CONDITION | | 1 | Good | 4 | Fair | 7 | Poor | Good | Fair | Poor | | | | | | | | | | |
| NO Defects | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| EXTENT | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| Low Med High | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 1 | 2 | 3 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 4 | 5 | 6 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 7 | 8 | 9 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| Low | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| Med | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| High | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| CONDITION | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 1 | Good | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 4 | Fair | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 7 | Poor | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| Good | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| Fair | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| Poor | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| <p>DRAINAGE</p> <div style="display: flex; align-items: center;">  <div style="margin-left: 10px;"> <table border="1" style="border-collapse: collapse; text-align: center;"> <tr><td colspan="2">CONDITION</td></tr> <tr><td>1</td><td>Good</td></tr> <tr><td>4</td><td>Fair</td></tr> <tr><td>7</td><td>Poor</td></tr> </table> </div> </div> <div style="display: flex; align-items: center; margin-top: 10px;"> <div style="writing-mode: vertical-rl; transform: rotate(180deg); font-weight: bold; margin-right: 5px;">SEVERITY</div> <table style="margin-right: 10px;"> <tr><td>Good</td></tr> <tr><td>Fair</td></tr> <tr><td>Poor</td></tr> </table> </div> | CONDITION | | 1 | Good | 4 | Fair | 7 | Poor | Good | Fair | Poor | <p>ROUGHNESS</p> <p style="font-size: small;">Check road for presence of the following:</p> <ul style="list-style-type: none"> - uneven surface - sags - humps - frost heaves <div style="display: flex; align-items: center; margin-top: 10px;"> <div style="writing-mode: vertical-rl; transform: rotate(180deg); font-weight: bold; margin-right: 5px;">SEVERITY</div> <table style="margin-right: 10px;"> <tr><td colspan="2">CONDITION</td></tr> <tr><td>1</td><td>Good</td></tr> <tr><td>4</td><td>Fair</td></tr> <tr><td>7</td><td>Poor</td></tr> </table> </div> <div style="display: flex; align-items: center; margin-top: 10px;"> <div style="writing-mode: vertical-rl; transform: rotate(180deg); font-weight: bold; margin-right: 5px;">SEVERITY</div> <table style="margin-right: 10px;"> <tr><td>Good</td></tr> <tr><td>Fair</td></tr> <tr><td>Poor</td></tr> </table> </div> | CONDITION | | 1 | Good | 4 | Fair | 7 | Poor | Good | Fair | Poor | | | | | | | | | | | | | | | | | | | | |
| CONDITION | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 1 | Good | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 4 | Fair | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 7 | Poor | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| Good | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| Fair | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| Poor | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| CONDITION | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 1 | Good | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 4 | Fair | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 7 | Poor | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| Good | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| Fair | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| Poor | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |

Appendix D

CAMP-RS Unpaved Condition Survey

CAMP-RS Unpaved Condition Survey

| | |
|-----------------------------------------------------------------------------------------------------------------------|---------------------------------------------------------------------|
| Street: _____ Distance: _____ Section #: _____ Start: _____ Start: _____ End: _____ End: _____ Length: _____ | Name: _____ Date: _____ Weather: _____ Temp (F°/C°): _____ |
|-----------------------------------------------------------------------------------------------------------------------|---------------------------------------------------------------------|

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| <h3>IMPROPER X-SECTION</h3>  <div style="display: flex; justify-content: space-between; align-items: flex-start;"> <div style="writing-mode: vertical-rl; transform: rotate(180deg); font-weight: bold;">SEVERITY</div> <table border="1" style="border-collapse: collapse;"> <tr><td>1</td><td>Good</td></tr> <tr><td>4</td><td>Fair</td></tr> <tr><td>7</td><td>Poor</td></tr> </table> </div> | 1 | Good | 4 | Fair | 7 | Poor | <h3>ROADSIDE DRAINAGE</h3>  <div style="display: flex; justify-content: space-between; align-items: flex-start;"> <div style="writing-mode: vertical-rl; transform: rotate(180deg); font-weight: bold;">SEVERITY</div> <table border="1" style="border-collapse: collapse;"> <tr><td>1</td><td>Good</td></tr> <tr><td>4</td><td>Fair</td></tr> <tr><td>7</td><td>Poor</td></tr> </table> </div> | 1 | Good | 4 | Fair | 7 | Poor | | | | | | | | | | | | | | | | | | |
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| 4 | Fair | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 7 | Poor | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 1 | Good | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
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| <h3>CORRUGATIONS</h3>  <div style="display: flex; justify-content: space-between; align-items: flex-start;"> <div style="writing-mode: vertical-rl; transform: rotate(180deg); font-weight: bold;">SEVERITY</div> <div style="text-align: center;"> <table border="1" style="border-collapse: collapse;"> <tr><td colspan="3">NO Defects</td></tr> <tr><td>Low</td><td>Med</td><td>High</td></tr> </table> <table border="1" style="border-collapse: collapse;"> <tr><td>1</td><td>2</td><td>3</td></tr> <tr><td>4</td><td>5</td><td>6</td></tr> <tr><td>7</td><td>8</td><td>9</td></tr> </table> </div> </div> | NO Defects | | | Low | Med | High | 1 | 2 | 3 | 4 | 5 | 6 | 7 | 8 | 9 | <h3>POTHOLES</h3>  <div style="display: flex; justify-content: space-between; align-items: flex-start;"> <div style="writing-mode: vertical-rl; transform: rotate(180deg); font-weight: bold;">SEVERITY</div> <div style="text-align: center;"> <table border="1" style="border-collapse: collapse;"> <tr><td colspan="3">NO Defects</td></tr> <tr><td>Low</td><td>Med</td><td>High</td></tr> </table> <table border="1" style="border-collapse: collapse;"> <tr><td>1</td><td>2</td><td>3</td></tr> <tr><td>4</td><td>5</td><td>6</td></tr> <tr><td>7</td><td>8</td><td>9</td></tr> </table> </div> </div> | NO Defects | | | Low | Med | High | 1 | 2 | 3 | 4 | 5 | 6 | 7 | 8 | 9 |
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| Low | Med | High | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
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| Low | Med | High | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 1 | 2 | 3 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 4 | 5 | 6 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 7 | 8 | 9 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| <h3>RUTTING</h3>  <div style="display: flex; justify-content: space-between; align-items: flex-start;"> <div style="writing-mode: vertical-rl; transform: rotate(180deg); font-weight: bold;">SEVERITY</div> <div style="text-align: center;"> <table border="1" style="border-collapse: collapse;"> <tr><td colspan="3">NO Defects</td></tr> <tr><td>Low</td><td>Med</td><td>High</td></tr> </table> <table border="1" style="border-collapse: collapse;"> <tr><td>1</td><td>2</td><td>3</td></tr> <tr><td>4</td><td>5</td><td>6</td></tr> <tr><td>7</td><td>8</td><td>9</td></tr> </table> </div> </div> | NO Defects | | | Low | Med | High | 1 | 2 | 3 | 4 | 5 | 6 | 7 | 8 | 9 | <h3>LOOSE AGGREGATE</h3>  <div style="display: flex; justify-content: space-between; align-items: flex-start;"> <div style="writing-mode: vertical-rl; transform: rotate(180deg); font-weight: bold;">SEVERITY</div> <div style="text-align: center;"> <table border="1" style="border-collapse: collapse;"> <tr><td colspan="3">NO Defects</td></tr> <tr><td>Low</td><td>Med</td><td>High</td></tr> </table> <table border="1" style="border-collapse: collapse;"> <tr><td>1</td><td>2</td><td>3</td></tr> <tr><td>4</td><td>5</td><td>6</td></tr> <tr><td>7</td><td>8</td><td>9</td></tr> </table> </div> </div> | NO Defects | | | Low | Med | High | 1 | 2 | 3 | 4 | 5 | 6 | 7 | 8 | 9 |
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| 7 | 8 | 9 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| <h3>DUST</h3>  <div style="display: flex; justify-content: space-between; align-items: flex-start;"> <div style="writing-mode: vertical-rl; transform: rotate(180deg); font-weight: bold;">SEVERITY</div> <table border="1" style="border-collapse: collapse;"> <tr><td colspan="2">CONDITION</td></tr> <tr><td>1</td><td>Good</td></tr> <tr><td>4</td><td>Fair</td></tr> <tr><td>7</td><td>Poor</td></tr> </table> </div> | CONDITION | | 1 | Good | 4 | Fair | 7 | Poor | <h3>ROUGHNESS</h3> <p>Check road for presence of the following:</p> <ul style="list-style-type: none"> - uneven surface - sags - humps - frost heaves <div style="display: flex; justify-content: space-between; align-items: flex-start;"> <div style="writing-mode: vertical-rl; transform: rotate(180deg); font-weight: bold;">SEVERITY</div> <table border="1" style="border-collapse: collapse;"> <tr><td colspan="2">CONDITION</td></tr> <tr><td>1</td><td>Good</td></tr> <tr><td>4</td><td>Fair</td></tr> <tr><td>7</td><td>Poor</td></tr> </table> </div> | CONDITION | | 1 | Good | 4 | Fair | 7 | Poor | | | | | | | | | | | | | | |
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| 4 | Fair | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 7 | Poor | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| CONDITION | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 1 | Good | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
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